



Johnson Ferry Road Design Guidelines



March 2013

Cobb County, Georgia



Cobb County...Expect the Best!

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A. Introduction

In 2011, the Cobb County Planning Commission and Board of Commissioners approved the Johnson Ferry Road Urban Design Plan. The purpose of the design plan was to establish a future road layout that will determine how the public, private, and non-profit sectors can work together to improve the functionality and aesthetics of this highly traveled north-south thoroughfare. The improvements to the corridor were based upon a model of implementing a “complete street” and were split into short-, mid-, and long-term improvements. One of the recommendations of the study is to develop a series of design guidelines that will be used to ensure that new investments along the corridor are occurring in a manner, where feasible, to support the long-term objectives of the design plan.

□ Urban Design Guidelines Boundary

The boundaries of the Johnson Ferry Design Guidelines span the length of Johnson Ferry Road from the Roswell Road-Johnson Ferry Road commercial activity node south to the Chattahoochee River. Per the Design Plan, the corridor guidelines are subdivided into four distinct districts based on the existing and desired development character for the various segments of the corridor (See Street Improvement - Short Term map in page 62). Specific standards are applied to each district to differentiate infrastructure and development along the corridor. The characters of districts are described below.

□ Character of Four Districts

The corridor is divided into four major districts, each with a unique character. The streetscape design in each district varies due to these different characteristics. These character descriptions are the specified detailing preferred road frontages, i.e. the intersection of the public and private realms, and do not intend to make any land use or zoning recommendation for individual properties. Any changes regarding zoning and land use need to be submitted to Cobb County for approval and will be evaluated based upon the categories identified in the future land use map.

D1 - Commercial/Retail District:

- Urban character – small blocks, buildings close and front to street, pedestrian friendly facilities, convenient public gathering spaces
- Highest commercial intensity of uses among the four districts
- Slip lanes design to separate pass through traffic from local traffic along Johnson Ferry Road
- Parking is allowed on street in association with the slip lanes or behind buildings
- Mainly use concentric street pattern in the Activity Center (Node I) area, intersection of Roswell Road and Johnson Ferry Road, combined with internal link street pattern on the west, east, and south along two roads as described in the Johnson Ferry Urban Design Plan
- Green space becomes a continued system along the streets (green trail) and in the center of shopping plazas (green pockets, parks) to connect and provide safe pedestrian accessibility



Introduction

D2 - Office/Institutional District:

- Enhance the existing suburban character and create a green visual corridor. Open spaces, canopies, and landscape buffering along Johnson Ferry Road
- One row of parking space between the front of the buildings and Johnson Ferry Road with a landscape buffer screening the parking lot from the street. Other parking located to the side or rear of the building
- Mainly apply to Internal Link street pattern to string up the linear developments on both sides of the street and provide better connectivity between institutional facilities as described in the Johnson Ferry Urban Design Plan

D3 - Mixed-use District:

- Town Center character – small blocks, buildings closer to street than D1. Retail, office, and residential mixed-use properties serving the surrounding neighborhood
- Slip lane with on-street parking design as found in D1 on Johnson Ferry Road and on Lower Roswell Road
- Mixed-use development encouraged to be vertical (single building mixed-use with shopping at street level) or horizontal (different use buildings mixed together in a uniform development plan). Also adding housing in this district will support the commercial uses while providing housing opportunities for older adults. This creates a buffer between more intense commercial uses along Johnson Ferry Road and stable single family residential neighborhoods
- Concentric Street pattern in the Town Center (Node II) area to increase the connection of all commercial uses and ease the traffic congestion in the intersection of Lower Roswell Road and Johnson Ferry Road as described in the Johnson Ferry Urban Design Plan. Integrate public gathering place, parks, and green spaces to meet the need for different activities and users.

D4 - Residential/Recreational District:

- Ensure public and private investment maintains quality residential surroundings
- Prevent strip commercial activities from extending into the district
- Landscaping is encouraged along Johnson Ferry Road to buffer surrounding residential communities from street traffic and noise
- Large street canopies are encouraged to provide shade along sidewalks
- Preserve existing street pattern so as to not disrupt the stable residential nature of the area

☐ **Phasing**

Phase I is the short term or midpoint improvement, which will be accomplished through the design guidelines herein. Phase II is the long term improvement, which includes the major infrastructure retrofit of Johnson Ferry Road. The short term or midpoint improvements do not require new public investment; it can be done through the county and the development community, as well as, community partnerships. The long term improvements will require detailed engineering and cost estimates prior to the Board of Commissioners making decisions on future investments of this nature.

□ **Applicability**

The following design guidelines are intended to assist architects, engineers, planners, developers and community members to make more informed design decisions based on community preference.

The design guidelines are to be applied to all new development and major redevelopment within the Johnson Ferry Urban Design Boundary as shown in the Street Improvement - Short Term map in page 62, unless otherwise specified. These design guidelines shall be applied to the review and approval of site plans and rezoning applications whenever rezoning related to new projects or redevelopments are being considered. The Johnson Ferry Design Guidelines should also be used as a guide to the Department of Transportation when making transportation related investment decisions along the corridor. However it is not the intent of this document for the DOT to install streetscape, lighting or other features not related to transportation.

Whenever the requirements of the Guidelines impose a more restrictive standard than the provisions of any other design guidelines, the requirements of the Johnson Ferry Design Guidelines shall take precedence. If any guideline element is in conflict with County's ordinances and regulations, a determination will be made as to which requirement will be enforced.

The guidelines can be flexible, as long as any exceptions are approved by Cobb County Community Development and are compatible with the north commercial and south residential theme of the corridor.

B. Intent and Goals of the Design Guidelines

The implementation of the design guidelines will achieve the goal of the short term street improvement to help provide a broad scope and direct physical and visual changes that gives Johnson Ferry Road a cohesive and distinctive look and identity for the future. The design guidelines will apply to all new development and major redevelopment in the study area. The property owner will be responsible for installing and maintaining all features described below. In addition, the design guidelines are a guide to assist community non-profit organizations on how they can be involved in the implementation process of the preferred urban design framework.

The guidelines lay out a concept for the street; identify street geometrics; and a palette of materials. The intent is to provide consistency where needed, but to also allow flexibility for designers within the palette of materials, since part of the street's character results from the mix of materials and uses over time. In addition, the guidelines provide recommendations for architectural treatments of public and private buildings or structures.

The design guidelines are the short term , or first step, to provide the following:

- A smooth transition from existing condition to desired future long term plan;
- Achieve and maintain a unified and pleasing aesthetic/visual quality in landscaping and streetscaping;
- Develop a distinct identity and street character for the corridor;



Streetscape Design Guidelines

- ☐ Provide a safe, accessible and aesthetically pleasing pedestrian and automobile environment;
- ☐ Create a consistent public environment within the Johnson Ferry Corridor; and
- ☐ Preserve and expand the open public space, green space and trail system.

Future street design (long term plan) will not be brought into practice in these design guidelines. Implementation of long term plan will require additional engineering, funding, and approvals. The purpose of presenting both short term and long term street designs schema below is to provide a clear definition and outline how these portions of street can be transformed step by step.

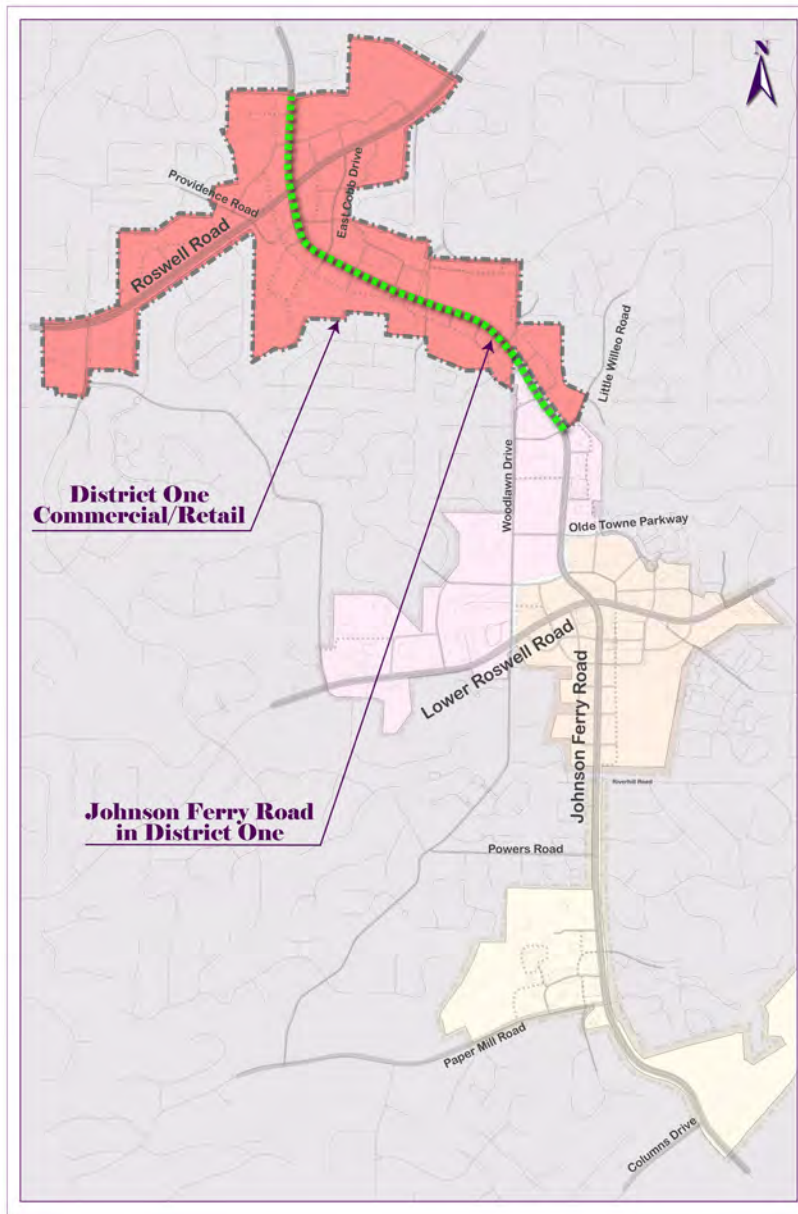
This document is to be used in conjunction with the Johnson Ferry Road Urban Design Plan Final Report.

C. Streetscape Design Guidelines for Johnson Ferry Road and Lower Roswell Road

The Streetscape Design Guidelines shall apply to the portion of Johnson Ferry Road and Lower Roswell Road and all private properties directly fronting to Johnson Ferry Road and Lower Roswell Road within the study boundary.

D1

District One: Commercial/Retail

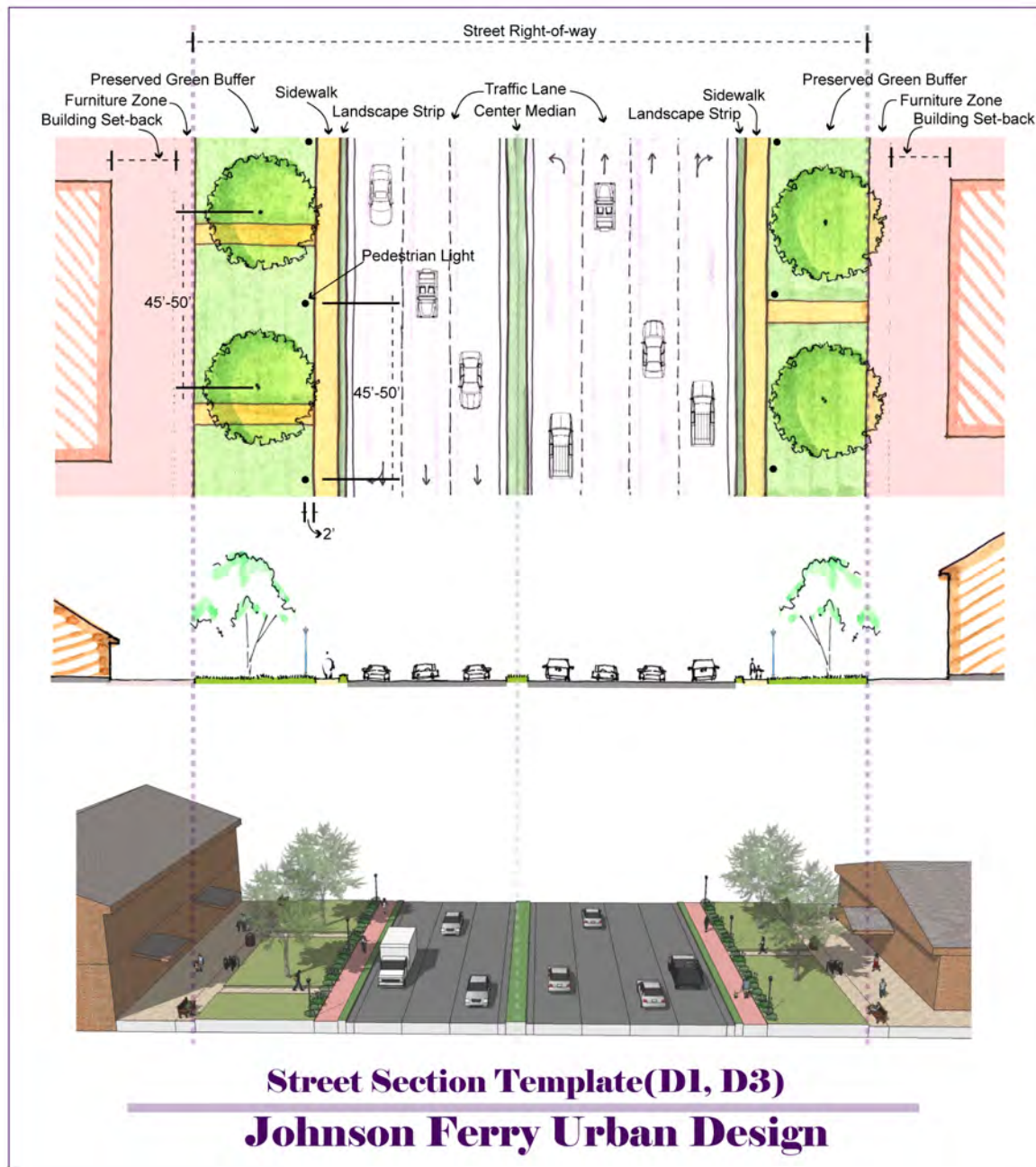


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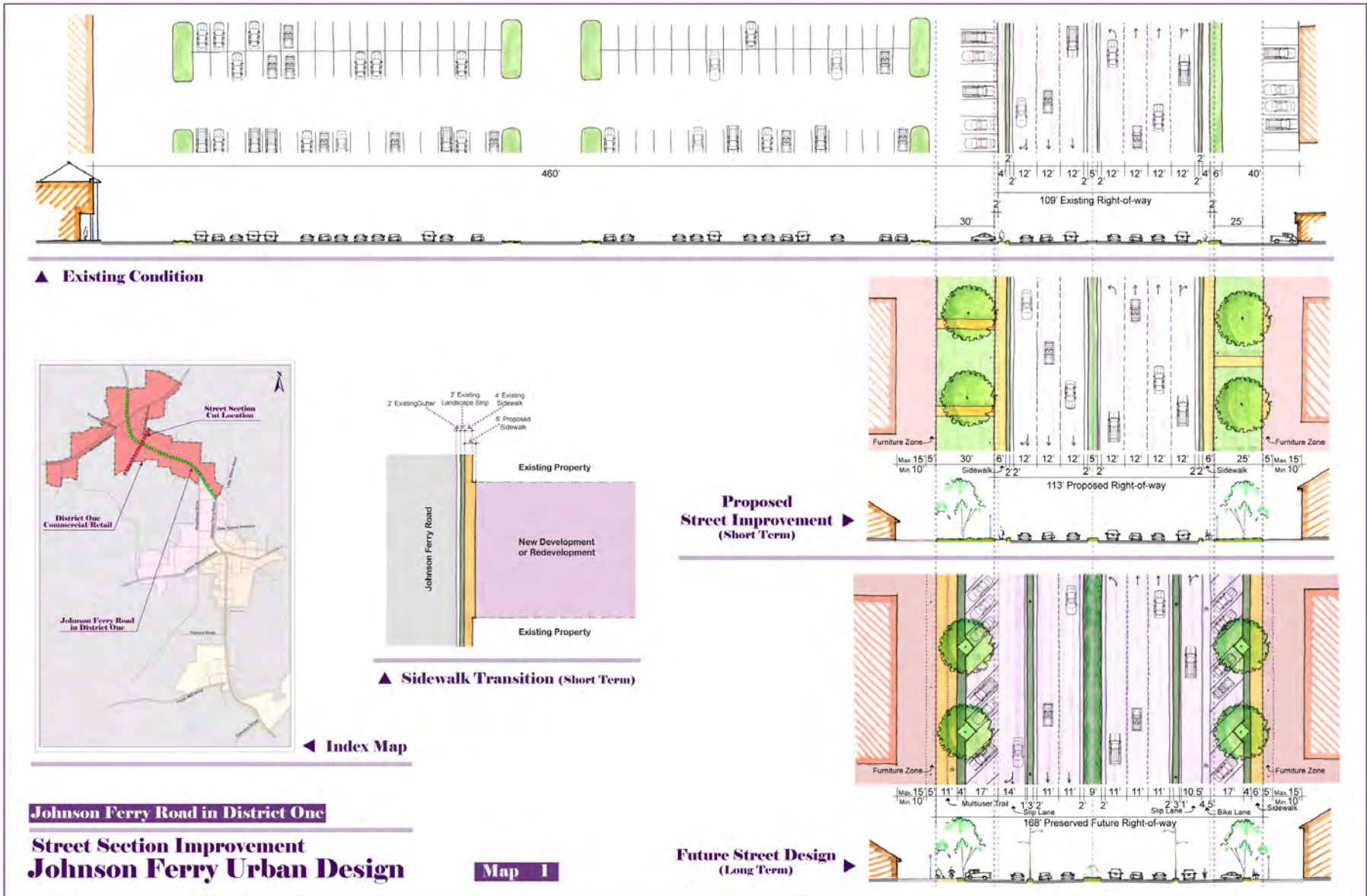


Streetscape Design Guidelines

1. Street Section Template



2. Street Section Design Graphics

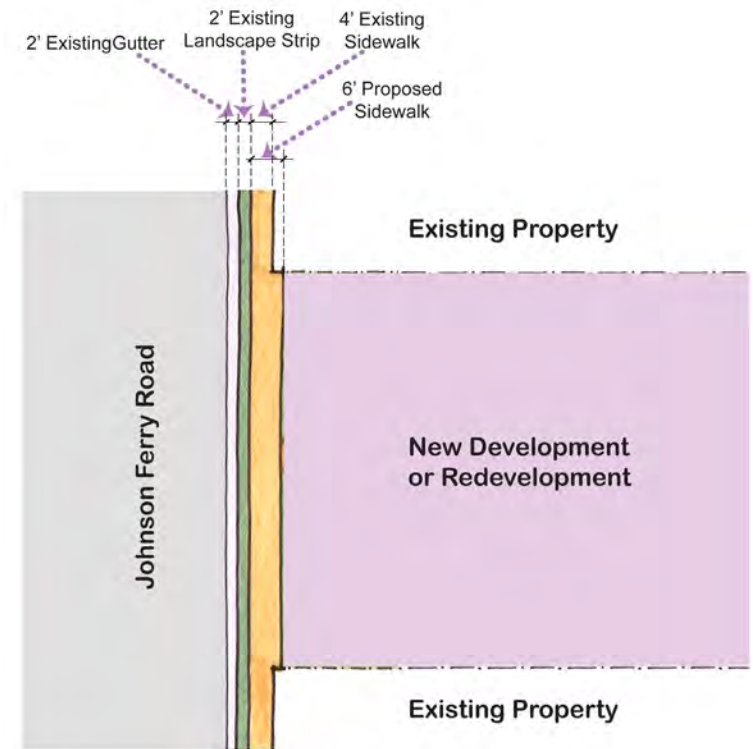




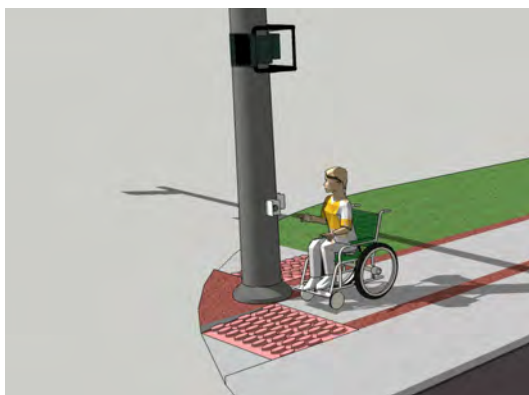
3. Pedestrian Facility

3.1 Sidewalk

- 3.1.1 Sidewalk shall be six (6) feet in width along the road frontages.
- 3.1.2 The location of the sidewalk is shown in the Street Section Template on page 8.
- 3.1.3 Concrete surface with DOT approved stamped concrete border to decorate and define the edges of the sidewalk is the suggested pavement style shown in the pictures below. Color shall be approved by the Community Development Agency and DOT for consistency.
- 3.1.4 If the sidewalk is in a good condition, stamped concrete should be added to extend the sidewalk two (2) feet behind the existing sidewalk. If the sidewalk is worn out or removed, a six (6) foot sidewalk should be installed.
- 3.1.5 Applying for ADA standard to meet the need of disabled individuals is required; such as, handicapped access curb cuts, blind path, etc. See images below as examples.
- 3.1.6 Sidewalk transition: the new sidewalk should have a smooth and continued transition or connection to the adjacent existing sidewalk. The recommended transitions are shown in the map beside.



Sidewalk Transition



Handicapped Access Curb Cut



Blind Path



Sidewalk Pavement



Streetscape Design Guidelines

3.2 Crosswalk

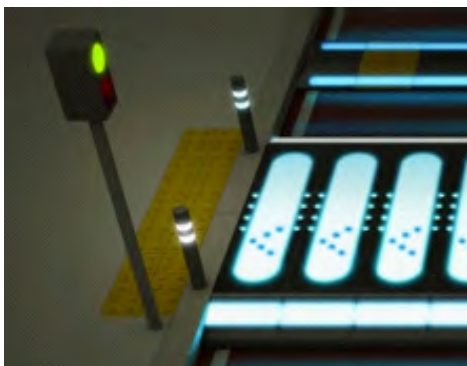
- 3.2.1 Intersections and the vehicle entrance/exit area crossing the sidewalk on Johnson Ferry Road should have a crosswalk. The block longer than 600 feet in distance may consider a midblock crossing which will need to be approved by the Cobb County Department of Transportation (Cobb DOT).
- 3.2.2 Convert the center green median to a pedestrian crossing median which creates a safety island for the crossing pedestrians.
- 3.2.3 Provide pedestrian activated crosswalk signals for all intersection segments at signalized intersections. Consider crossing technologies for unsignalized intersections with heavy pedestrian traffic which will need to be approved by Cobb DOT. Countdown pedestrian signals are encouraged to clearly indicate the signal time control and increase the safety for both pedestrians and vehicles.
- 3.2.4 Johnson Ferry Road Crosswalks should be stamped concrete in a consistent color with sidewalks and bordered by white stripes in accordance with the Manual on Uniform Traffic Control Devices (MUTCD). Crosswalks crossing other roads and parcel entrances on Johnson Ferry Road should be stamped concrete in warm color with brick pattern and white stripe boundaries. See Image details beside as examples. Crosswalk color and pattern need to be approved by Cobb County Community Development and DOT.
- 3.2.5 Encourage decorated poles or lights to define the space and the entrance of the crosswalk to enhance the visibility of pedestrian activities. The examples are below.



Safety Island



Crosswalk Pavement (crossing Johnson Ferry)



Crosswalk Entrance



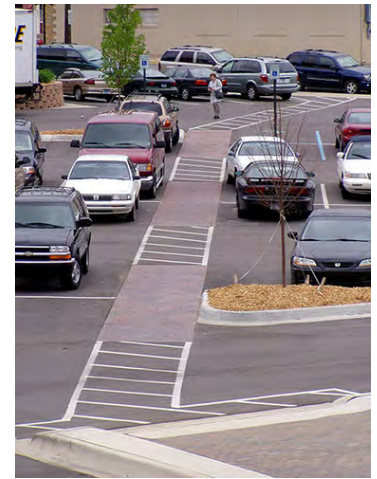
Crosswalk Entrance



Crosswalk Pavement (crossing other road)

3.3 Pedestrian Pathway

- 3.3.1 At least one minimum four (4) foot pedestrian path per non-residential buildings is needed to connect the street sidewalks with the entrance(s) of buildings;
- 3.3.2 The location of pedestrian paths should to be carefully selected to keep the paths as short as possible and to minimize conflicts with cars.
- 3.3.3 Matching the pavement style of sidewalks or crosswalks is recommended for the pedestrian path to create a consistent streetscape. See Image beside as a sample.
- 3.3.4 When the primary entrance of a building is more than 150 feet from the nearest point of a public sidewalk, and the entrance is accessed by a pathway traversing a parking lot, then an overhead shade structure or tree canopy is required along the pathway. Shade elements may include opaque structures (e.g., arbor, pergola, portico, awning, canopy, etc.) and/or shade trees planted with a minimum of one tree provided for every 25 feet of lineal pathway. The shade trees also can count towards parking lot tree placement requirements.



Pedestrian Pathway

4. Furniture Zone

The five (5) feet furniture zone is located behind the roadway right-of-way and in the front of the private properties. See Street Section Template on page 8.

The color, pattern, and the material of the pavement in furniture zones should be consistent with the nearby sidewalks.

Benches, trash receptacles, and bike racks shall be placed in the furniture zone and close to the building entrance area. The Bench and the trash receptacle need to be put together as a group with no more than 100 feet spacing between each group. If the property contains less than 100 feet of frontage and more than 50 feet, a minimum of one bench and one trash receptacle is required.

The recommended furniture shows in the picture below. A similar style with equivalent quality can be used and need to be approved by Cobb County Community Development.

The installation and maintenance of the furniture zone is the responsibility of private property owners or tenants of the commercial space.

4.1 Benches

Benches are essential to making a sidewalk pedestrian-friendly.

- 4.1.1 Benches need to be placed parallel to the sidewalk facing to the street. When placed in bulb-outs at intersections, benches can be placed perpendicular to the sidewalk.
- 4.1.2 Benches longer than four (4) feet should include a center or intermediate arm to separate the bench into two segments.
- 4.1.3 Benches will be made of 100% recycled plastic boards in dark green color with black cast aluminum bench ends.



Bench



Streetscape Design Guidelines

4.2 Trash Receptacles

Trash receptacles should have inner steel frame, dark green 100% recycled plastic slats around the outside and steel flat top with ash urn or rain guard.

4.3 Bike Racks

4.3.1 At least one three-bike rack will be required by each commercial property.

4.3.2 Large properties with more than one major entrance should have a three-bike rack at each entrance.

4.3.3 Bike racks should be made of continuous sinuous curving pieces of metal painted in green.

4.3.4 Bike rack art is recommended and will make a great contribution to the street beautification.

4.4 Other street furniture, such as planters, bollards, decorative sign post, if provided, shall be in black color and metal material. The style should be consistent with the lighting fixtures.

5. Lighting

5.1 Street Lighting

The improvement of the Street lighting will be facilitated during the long term development period.

5.2 Parking Lot Lighting

Parking lot lighting will aid both pedestrians and motorists by creating a safe, well lit environment and add to the theme of the corridor. The lighting style should be selected in conjunction with the pedestrian lights on the street. Lighting pole should be metal material and in black color.

5.3 Pedestrian Lighting

5.3.1 Location

- Pedestrian lights should be installed two (2) feet away from the sidewalks and in the preserved green buffers.
- Pedestrian lights need to be equally spaced between street trees and be installed 45 to 50 feet on-center.

5.3.2 Breakaway bases for lighting poles are required.

5.3.3 Rather than installing individual pedestrian lights, applicant should enter into the Johnson Ferry Road Pedestrian Street Light District. Cobb County DOT and Georgia Power will install and maintain pedestrian lights in accordance with the county's pedestrian street light district ordinance. Once approvals of ¼ mile of property owners are obtained, the pedestrian street light district will be in effect.



Trash Receptacles



Bike Racks



Pedestrian Light



Downloaded from <http://ajph.org/> on November 10, 2015

Sitescape®

SITE • ROADWAY • AREA LIGHTING

NEWPORT

33300BP - 33330BP

FIXTURE SELECTION

UV STABILIZED ACRYLIC OR POLYCARBONATE REFRACTORS TYPE III or V

2 1/2" MIN. LAMP LENGTH FOR 33300 SERIES

15 13/16" Ht. 17" ID.

33300BP

17 13/16" Ht. 17" ID.

33330BP

EXFURE NO.
MAX. WATTS -
INC.
MH
HPS
MH
EPA

220W
250W
350W
400W
2,110

250W
250W
350W
400W
2,110

FEATURES

SPECIFICATIONS

MODEL No. shown 33330BP

E.P.A. - 2,110
Max. Wt. 31 lbs

Clear acrylic refractor (polycarbonate UV stabilized, Type III or V refractor globe, (customer must specify).

Porcelain mould base - dry socket. Medium base 4kw socket available.

Three 1/4-20 s.s. set screws & hex nuts supplied for locking globe to ballast pod. Loosen all three set screws and hex nuts for replacement.

Two pc. cast aluminum water tight reflector compartment. Lower housing has slanted flutes. (CP Ballast selection - see chart below).

Three 1/4-18 s.s. set screws & hex nuts supplied for securing ballast to post or tenon.

Filter slots over 5" dia. post or tenon.

PHOTOMETRIC DATA

Symmetric Distribution

Fixture: 33330BP
Lamp: 150W H.P.S.
Lamp Lumens: 16,000

Optics: Type V
Mounting Height: 12'
Globe: Polycarbonate refractive

SOURCES: (factory installed, prewired and tested).

INC. - (incandescent) 250 watt max.

MV - (mercury vapor) 50-75-100-175-250 watt.

S - (high pressure sodium) 35-50-70-100-150-250 watt.

MH - (metal halide) 70-100-175-250 watt.

FINISHES:

All Hanover Lantern finishes available, see page 3 for selection.

EXFURES:

33300BP

33330BP

GLOBES: (customer must specify).

CA-III - Clear acrylic refractive globe, Type III (max. watts: INC., MV, MH-175W, HPS-150W)

CA-V - Clear acrylic refractive globe, Type V (max. watts: INC., MV, MH-175W, HPS-150W)

CP-III - Clear polycarbonate refractive globe, Type III (max. watts: INC., MV, MH-175W, HPS-150W)

CP-V - Clear polycarbonate refractive globe, Type V (max. watts: INC., MV, MH, HPS-250W)

VOLTAGES:

120-200-240-277 - QUAD (contact factory for availability)

PHOTO CONTROL: (optional) factory installed.

No. 10-120 volt only.

No. 11-200 to 277 volt. (Must be installed in post only.)

ORDERING GUIDE

To ensure prompt and efficient processing of your order, Please follow the sequence listed in the example below.

- 1 SOURCE:**
INC. - incandescent
MV - mercury vapor
S - high pressure sodium
MH - metal halide
- 2 FIXTURE NO.:**
33300BP
33330BP
- 3 GLOBES:**
CA-III - Clear acrylic refractive globe, Type III
CA-V - Clear acrylic refractive globe, Type V
CP-III - Clear polycarbonate refractive globe, Type III
CP-V - Clear polycarbonate refractive globe, Type V
- 4 FINISH:**
See page 3 for selection
- 5 WATTAGE:**
MV-50-75-100-175-250
S-35-50-70-100-150-250
MH-50-70-100-175-250
- 6 VOLTAGE:**
120V-200V-240V-277V
- 7 PHOTO CONTROL:** (optional)
No. 10 - 120 volt only
No. 11 - 200/277 volt.

1 33300BP CA-III WBZ 100 120 10
For post selection, see tabbed section P.
Accessories selection, see tabbed section A.

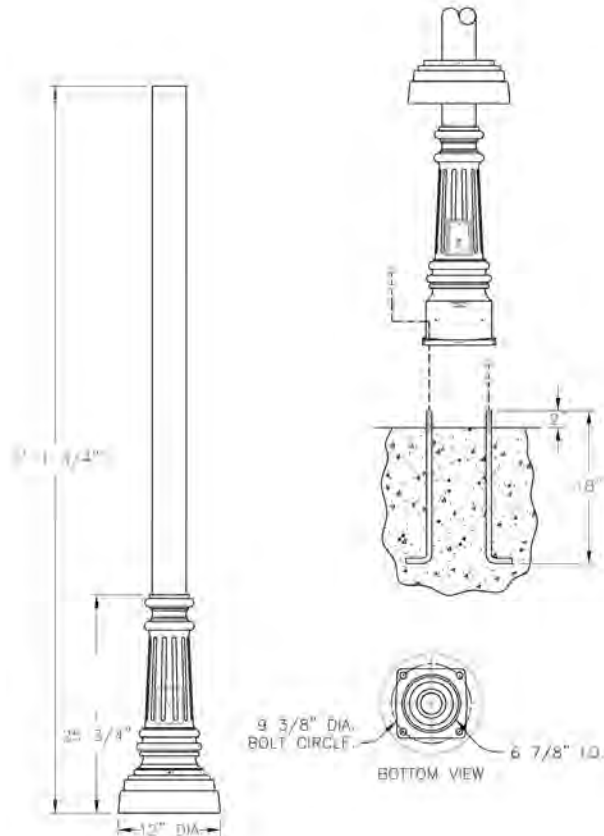
350 Kindig Lane, Hanover, PA 17331
Phone: (717) 632-4464
Fax: (717) 632-5059
Email: sales@hanoverlantern.com
Web: www.hanoverlantern.com

Hanover
LANTERN
Manufactured in the USA

Hanover Lantern is a Philips group brand

G-20

PHILIPS



Ordering Guide

Example: 289- 6 ABS 3 B 1

Product Code	289-	Anchor Base Post
Height(s)	6 8 10 12	(6 ft.) (8 ft.) (10 ft.) (12 ft.)
Finish	ABS ACP ARD ASI BLK BRN BRZ FGN GRA IRN RBZ SRT VBZ VCP VGN VTC WBZ WHT	Antique Brass Antique Copper Antique Red Antique Silver Black Brown Bronze Forest Green Granite Ironstone Rustic Bronze Shadow Rust Verde Bronze Verde Copper Verde Vintage Copper Weathered Bronze White
Tenon Option	3 4	3" dia. 4" dia.
Photoeye (Optional)	B C D	120V 208V - 277V 120V only (for 3" OD Post)
Outlet (Optional)	1 2 D	4" from Top of Base 12" from Top of Pole For Direct Burial & Anchor Base

Specifications

CONSTRUCTION:

Cast aluminum anchor base. Cast aluminum removable access door. Straight post welded to base. Base cover included with post.

FINISH:

Resilient TGIC thermoset polyester powdercoat paint is electrostatically applied to every fixture. Specially formulated for Hanover Lantern, it provides UV protection, and the highest temperature rating in the industry. In addition to the standard color choices shown, a spectrum of custom colors is available.

WARRANTY:

Three-year limited warranty.

Tenon/Top:

4" O.D.

Bolt Circle:

9 3/8" dia.

Anchor Rods:

(4) 1/2" dia. x 18" long zinc plated steel

Pedestrian Light - Pole and Base



6. Landscaping

All landscaping within right of way will require approval of Cobb DOT.

6.1 Center medians between opposite traffic lanes and in the center of the road

- 6.1.1 Convert five (5) foot center concrete medians into landscaped median covered with grass and/or shrubs
- 6.1.2 The center median will become a safety island when intersecting with the crosswalk (See Safety Island image on page 12).

6.2 Landscape Strip

- 6.2.1 Two (2) foot planting strips are located between curb or edge of pavement and sidewalk.
- 6.2.2 Planting strips shall be landscaped with grass or low flowering perennial shrubs.

6.3 Preserved Green Buffers Behind Sidewalks

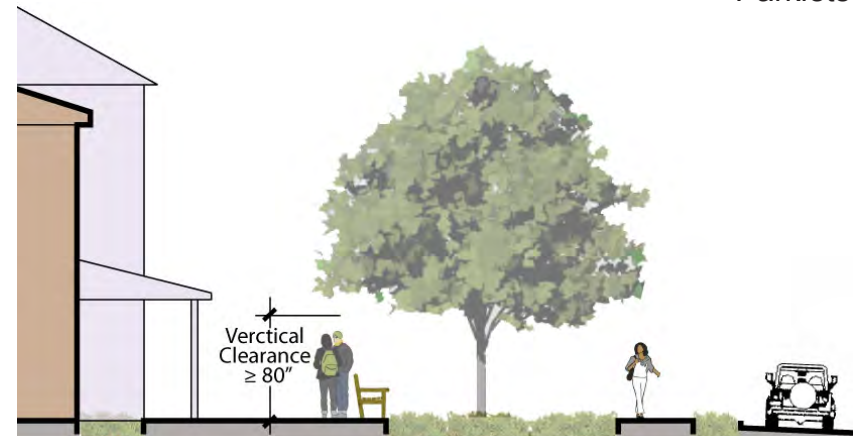
- 6.3.1 A 30 foot preserved green buffer is behind the sidewalk on west side of the Johnson Ferry Road, and 25 foot buffer is on east of the road for the purpose of not only creating more green space along streets in the short term but also preserving the space for future street right-of-way.
- 6.3.2 Grasses and street trees should be planted in the buffer. Bushes and flowers are also encouraged in the area (See section 6.6 for planting species details on page 19).
- 6.3.3 In the short term, temporary parking will be allowed as the method of compensating loss of existing parking spaces in the building front.
- 6.3.4 Parklets are encouraged in the buffer area to provide a public gathering place, especially in front of a restaurant, café or coffee house. Parklets can be temporary and just during the short term or seasonal.

6.4 Street Tree

- 6.4.1 Street trees shall be located within the preserved green buffers and 15 feet behind sidewalks.
- 6.4.2 Street trees shall be planted with 45 to 50 feet equal separation on both sides of the street. They should be equally spaced between street lights (See Street Section Template on page 8).
- 6.4.3 The minimum vertical clearance of tree branches is 80 inches.



Parklets



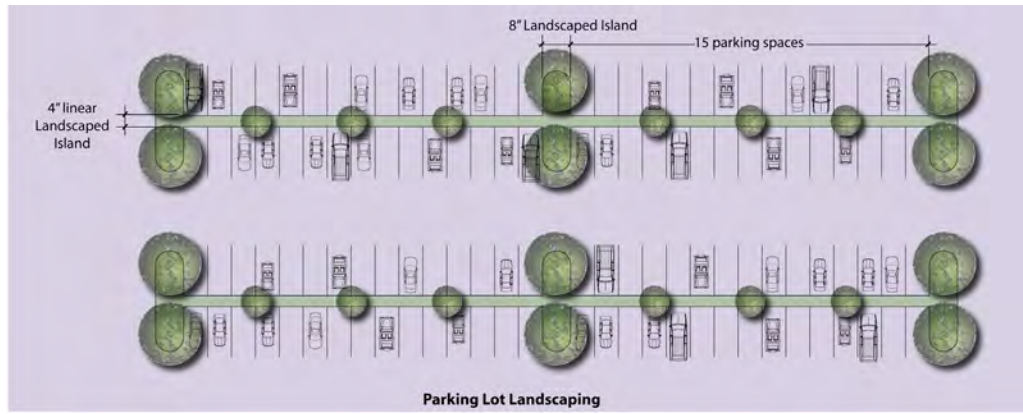
Street Tree Vertical Clearance



Streetscape Design Guidelines

6.5 Parking Lot Landscaping

- 6.5.1 Parking lots adjacent to residential properties or visible from public right-of-ways should be landscaped and screened from view using shrubs and smaller trees;
- 6.5.2 Landscaped islands shall be provided throughout all surface-parking area.
 - A minimum of eight (8) feet wide of a parallel landscaped island shall be provided between each 15 parking spaces, please see the figure below as reference. At least one tree is required in each landscaped island.
 - The minimum four (4) feet linear landscaping island should be placed between rows of parking spaces. Small trees and bushes are recommended in the island (See the figure below as reference).
 - These landscaping islands are also suggested between automobile and pedestrian spaces for buffering purposes.
 - Pervious surface parking is encouraged at any surface parking areas. See the picture below as reference.



Landscape Island in Parking Lot



Pervious Parking

6.6 Tree species and planting techniques should be selected to create a unified image of the street. Recommended plant species are:

6.6.1 Large Street Trees: (Must be planted outside AASHTO clear zone)

<i>Name</i>	<i>Height X Spread</i>	<i>Description</i>
Willow Oak	40'X30'	rounded shape, yellow fall color
Lacebark Elm	40'X30'	vase shaped, grows quickly
Princeton Elm	60'X40'	vase shaped, yellow fall color
Ginkgo	50'X30'	yellow fall color, specimen
Red Maple	50'X30'	red fall color, grows quickly



Willow Oak



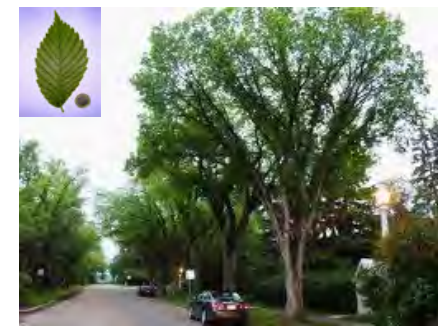
Lacebark Elm



Red Maple



Ginkgo



Princeton Elm



Streetscape Design Guidelines

- 6.6.2 Landscape Buffer (in the front of the Preserved Green buffer, between temporary parking lots and the street):
Sight distance for all transportation users shall be verified prior to installation.

<i>Name</i>	<i>Height X Spread</i>	<i>Description</i>
Needlepoint Holly	8'X10'	evergreen, red berries during winter
Indian Hawthorn	3'X5'	evergreen, white flowers in spring,
Inkberry	6'X8'	evergreen, black berries during winter
Juniper	6'X8'	evergreen, mass plantings, slopes
Barberry	6'X6'	evergreen, thorns, rich green foliage



Needlepoint Holly



Indian Hawthorn



Barberry



Juniper



Inkberry

6.6.3 Landscape Buffer (between the street and residential properties):

<i>Name</i>	<i>Height X Spread</i>	<i>Description</i>
Little Gem Magnolia	40'X15'	evergreen, large white flowers spring through fall
Nellie R Stevens Holly	20'X15'	evergreen, pyramidal shape, red berries
Loropetalum	8'X10'	evergreen, rounded, purple leaves, pink flowers
Tea Olive	8'X8'	evergreen, very fragrant white flowers in late summer



Little Gem Magnolia



Tea Olive



Loropetalum



Nellie R Stevens Holly



Streetscape Design Guidelines

6.6.4 Small Trees (strip between parking lot and roadway or building, strip in parking lot):

<i>Name</i>	<i>Height X Spread</i>	<i>Description</i>
Crape Myrtle	20'X20'	white, pink, lavender, or red flowers in summer
Trident Maple	20'X20'	rounded, yellow or red fall color
Bosque Elm	50'X40'	very upright, good for narrow spaces
Zelkova	60'X60'	upright vase shape, yellow or red fall color
Redbud	25'X15'	oval shape, purple flowers in early spring
Goldenrain tree	20'X10'	rounded shape, yellow flowers in spring
Flowering Cherry	30'X25'	upright, pink to white flowers in spring



Crape Myrtle



Flowering Cherry



Trident Maple



Goldenrain Tree



Redbud



Zelkova



Bosque Elm

6.6.5 Flowering perennial shrubs for aesthetic improvements:

<i>Name</i>	<i>Description</i>
Knockout Rose	many varieties, size varies by variety, red, white, yellow flowers, blooms from spring through fall
Encore Azaleas	many varieties, size varies by variety, red, white, yellow flowers, blooms once in spring and once in fall
Dianthus	6"h, perennial, bright pink flowers in spring
Phlox	upright varieties, 4'X3', perennial, pink or white flowers in summer
Lantana	upright varieties, 5'X10', perennial, multicolored flowers in summer
Wild Indigo	5'X5', perennial, blue, yellow, or white flowers in spring, black seed pods in summer
Verbena	6"X3', perennial, bright purple flowers in late spring
Salvia	20"X3' perennial, white or purple flowers on upright spikes from early summer until late fall
Daylily	many varieties, size varies by variety, red, white, yellow flowers during summer



Knockout Rose



Encore Azaleas



Salvia



Verbena



Lantana



Daylily



Wild Indigo



Streetscape Design Guidelines

7. Parking

7.1 Primary parking space shall be located on the back or the side of buildings.

7.2 Store Front Parking

Pervious temporary parking in green buffer between big street trees is permitted as the compensation of losing existing store front parking. When the streets are widened through long term retrofitting the corridor the on-street parking will be provided to replace the store front parking. Only one row of the store front parking spaces shall be allowed. Porous grass paving grids shall be used to prevent the grass from being damaged by cars. See pictures below as reference.



Pervious Temporary Parking

7.3 Parking Structure

7.3.1 Parking deck and parking underground is encouraged to save the limited space and decrease impervious surfaces. Parking space also can be accommodated through mixed use parking structures such as building envelopes around parking decks or parking above commercial buildings.

7.3.2 Such structures must be architecturally compatible with the remainder of the development surrounding them, such as building façade style, material and colors.

7.3.3 The façade of the parking structure on the main street should be carefully designed to have a commercial building look. Parking deck entrance is recommended to be on the side or back of the building. See pictures below as reference.



Parking Structure



8. Utility Location

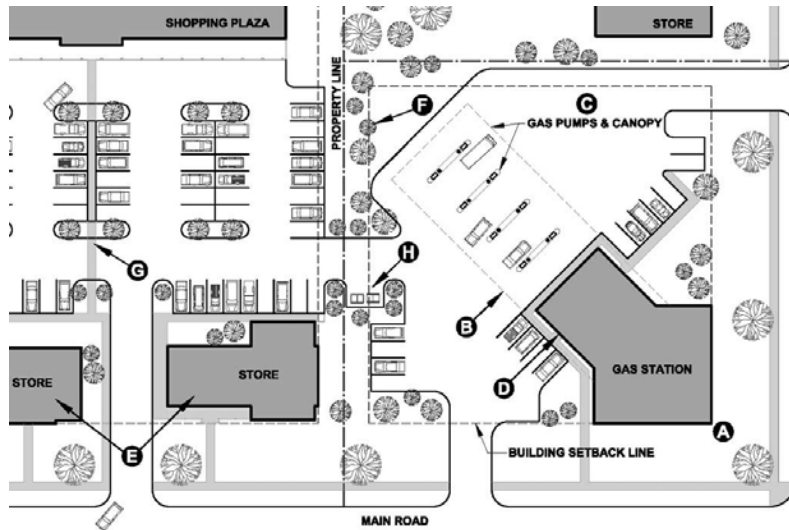
- 8.1 Utility line should be placed only on one side of the street.
- 8.2 In the long term, utility poles is recommended to be located in the 4 foot landscape strip between the sidewalk and the on-street parking.
- 8.3 Matching the material of utility poles to that of street lighting and pedestrian lighting is strongly recommended.
- 8.4 Utility lines crossing Johnson Ferry Road and connecting to each property should be buried underground.
- 8.5 Main utility lines are encouraged to be buried underground.

9. Building Requirements

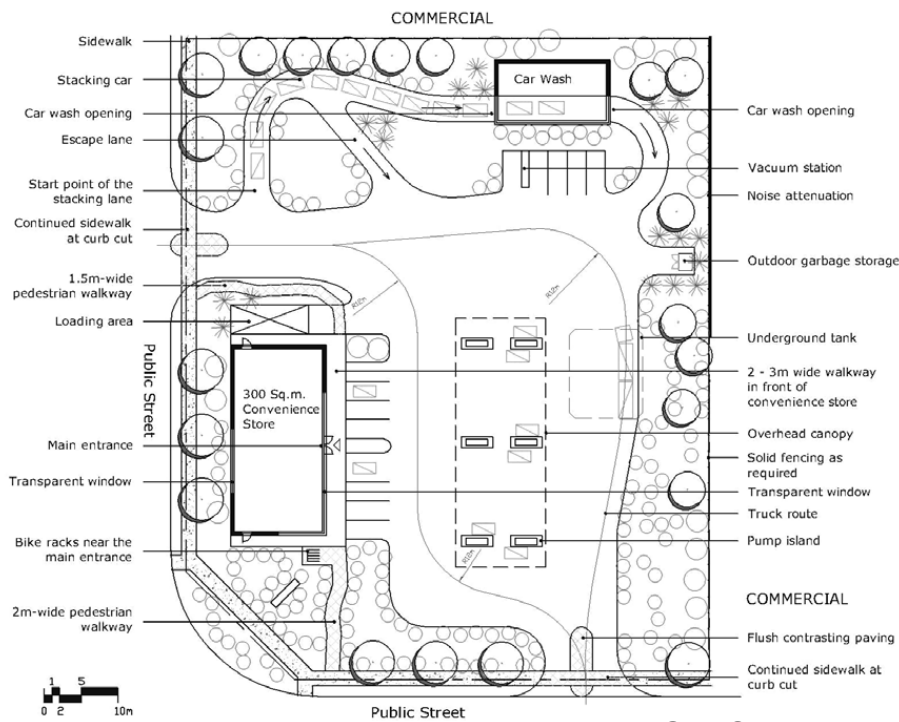
- 9.1 Building placement and the space between the furniture zone and the building
 - 9.1.1 The ten (10) to fifteen (15) foot building setback should be designed as the pedestrian walkway in front of buildings when it does not conflict with zoning requirements.
 - 9.1.2 The pattern and color of the paved pedestrian walkway in conjunction with that of sidewalk or multiuser trail is encouraged.
 - 9.1.3 It is recommended to plant small tree, bushes, grasses, or potted vegetation in front of the building to incorporate streetscape changes and provide more green space and pervious surface along the long continued walkway.
 - 9.1.4 Use of canopies and awnings on storefront and office windows is permitted and strongly encouraged. Canopies and awnings should be at least 90 inches in height as the minimum vertical clearance when they are above the walkway.
- 9.2 Architecture Standard – apply on new commercial buildings or major renovations
 - 9.2.1 It is required that the front of all buildings along Johnson Ferry Road be oriented toward the street. Building placement should consider site circulation and blend with the commercial characteristics of this district.
 - 9.2.2 It is preferred that new commercial developments be kept to a neighborhood scale. The building façade color and material should be consistent with the surrounding building environment. To prevent large vertical or horizontal blank expanses on façades, architectural detailing is encouraged. Expanses can be broken up with windows, bays, materials, extending or stepping back façade, front porches or patios, balconies, alternating rooflines, awnings, landscaping or other similar features.
 - 9.2.3 Building entrance should face to and be visible from the street and directly accessible from the pedestrian sidewalk area.
- 9.3 Car Related Business - gas station, car wash and car repair
 - 9.3.1 Position – apply on new buildings
 - The building mass or the store should be placed near or close to street or intersection
 - The gas pump, garage or car wash facility should be placed to the rear lot line and the back of the primary building. See the pictures in next page as reference.



Streetscape Design Guidelines



Enhanced visual experience and higher quality of streetscape



Gas Station Layouts



9.3.2 Buffer

Architectural walls, decorative fences, and planting should be used to enhance the streetscape and screen undesirable views along the street. See pictures below as examples.



9.3.3 Architectural

- The building should be designed as an integral part of the station architecture, and should be consistent with the surrounding building environment.
- Roofs of gas pump canopies should be sloped (i.e. not a flat roof). There shall be no illuminated panels on the canopy. Architectural treatments should be applied on the columns under the canopy in a manner that is complementary to the main building.



- Light weight or sail like canopies shall be used for the car wash facilities. Light, natural or semi transparent color is recommended. High contrast colors such as purple vs. yellow, red vs. green and pastels are not allowed to be used. The material, color and style of the canopy should be well connected to the environment created by surrounding buildings. Light color posts or columns are recommended. See below for canopy examples.

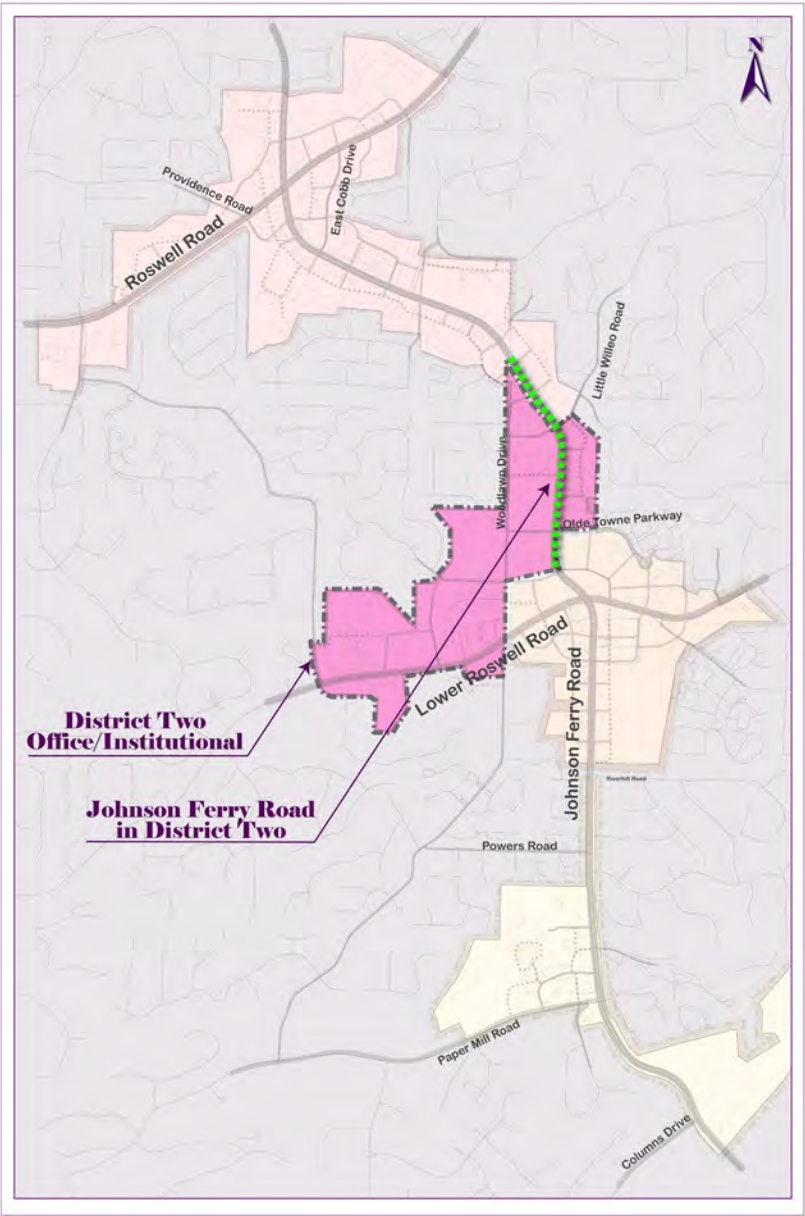


10. Residential Building Frontage

- 10.1 In front of residential buildings a minimum 25 foot building set back is required. A fence made of brick or stone on the residential property is highly recommended.
- 10.2 The front yard of the residential property should be well landscaped.
- 10.3 In spite of on-street parking which will not be required in front of residential properties, the street right of way design in front of residential properties should remain the same as that in front of commercial properties.

11. Institutional Building Frontage

On-street parking will not be required in front of institutional properties; other elements in the street section design should remain the same as that in front of commercial properties.



D2

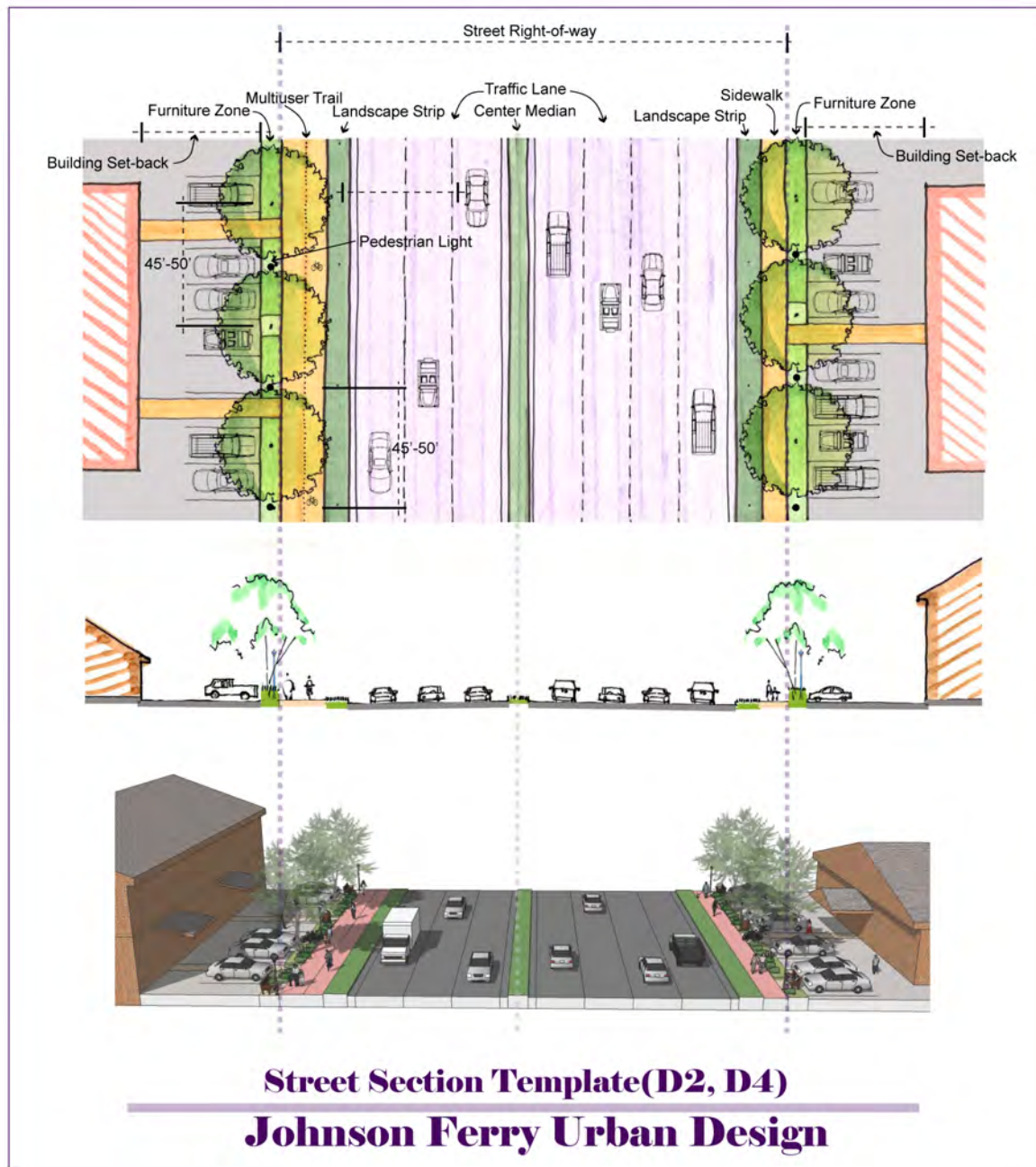
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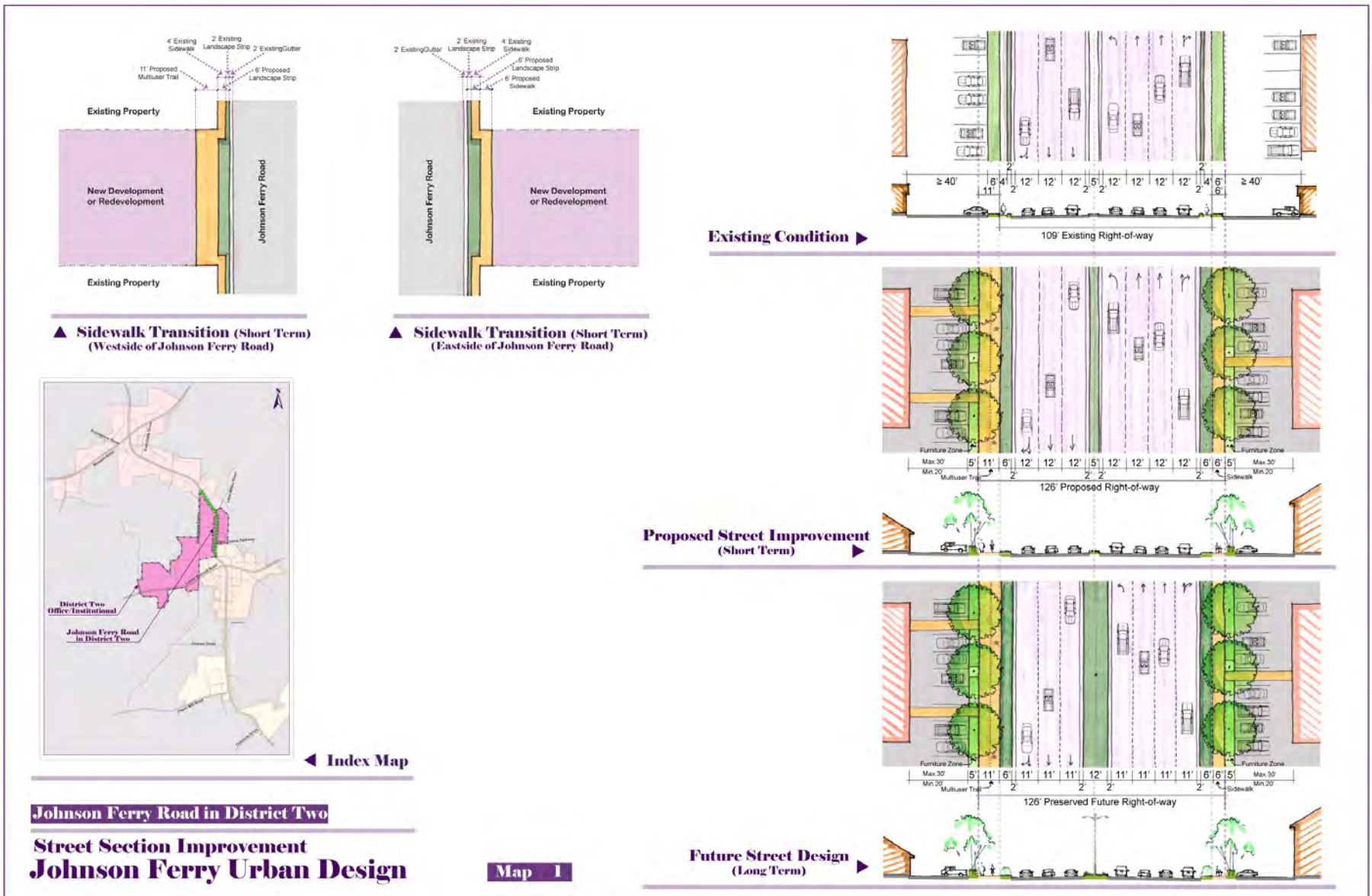


Streetscape Design Guidelines

1. Street Section Template



2. Street Section Design Graphics





Johnson Ferry Road in District Two

Street Section Improvement Johnson Ferry Urban Design

Existing Condition ▶



**Proposed
Street Improvement
(Short Term)** ▶



**Future Street Design
(Long Term)** ▶



Map 2



3. Pedestrian Facility

3.1 Sidewalk

- 3.1.1 Sidewalk shall be six (6) feet in width along the road frontages on east side of Johnson Ferry Road.
- 3.1.2 The location of the sidewalk is shown in the Street Section Template on page 30.
- 3.1.3 Concrete surface with DOT approved stamped concrete border to decorate and define the edges of the sidewalk is the suggested pavement style shown in the pictures below. Color shall be approved by the Community Development Agency and DOT for consistency.
- 3.1.4 Applying for ADA standard to meet the need of disabled individuals is required; such as, handicapped access curb cuts, blind path, etc. See images below as examples.
- 3.1.5 Sidewalk/multiuser Trail transition: the new sidewalk/multiuser trail should have a smooth and continued transition or connection to the adjacent existing sidewalk. The recommended transitions are shown in the map beside.



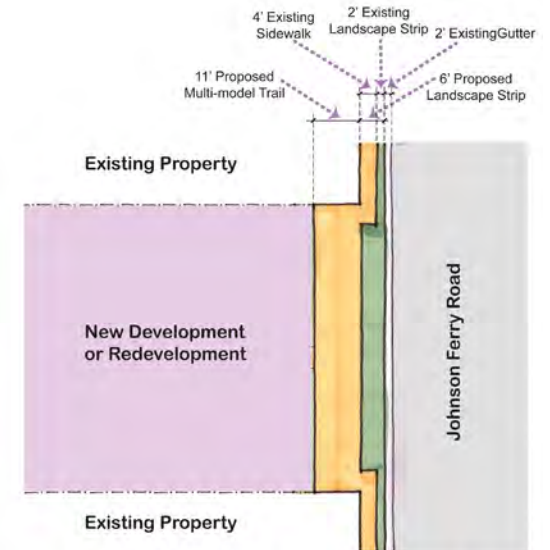
Blind Path



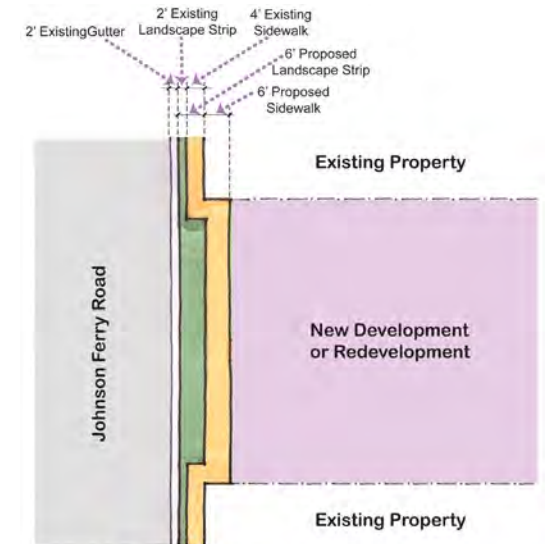
Handicapped Access Curb Cut



Sidewalk Pavement



D2 (Westside of Johnson Ferry Road)



D2 (Eastside of Johnson Ferry Road)

Sidewalk Transition



Streetscape Design Guidelines

3.2 Multiuser Trail

3.2.1 A eleven (11) foot multiuser trail for bicyclists and pedestrian shall be located on the west side of Johnson Ferry Road (See the Street Section Template on page 30 for the location).

3.2.2 The pavement style of the pedestrian walkway in the multiuser trial shall be same as the sidewalk on page 33 section 3.1.

3.2.3 The bike lane in the multiuser trail shall be paved or stamped in the same color as sidewalk borders to clearly define the biking space and per the MUTCD.

3.3 Crosswalk

The requirements are same as that in District One (See page 12).

3.4 Pedestrian Pathway

The requirements are same as that in District One (See page 13).

4. Furniture Zone

The five (5) foot furniture zone is located behind the street right-of-way and in the front of the private properties (See Street Section Template on page 30).

The furniture zone should be landscaped with grass. Street trees should be planted in this zone.

Benches, trash receptacles, and bike racks shall be placed in the furniture zone and close to the building entrance area. The Bench and the trash receptacle need to be put together as a group with no more than 100 feet spacing between each group. If the property contains less than 100 feet of frontage and more than 50 feet, a minimum of one bench and one trash receptacle is required.

The recommended furniture styles are same as those described in District One (See page 13-14).

The installation and maintenance of the furniture zone is the responsibility of private property owners or tenants of the commercial space.

5. Lighting

5.1 Street Lighting

The improvement of the Street lighting will be facilitated during the long term development period.

5.2 Parking Lot Lighting

Parking lot lighting will aid both pedestrians and motorists by creating a safe, well lit environment and add to the theme of the corridor. The lighting style should be selected in conjunction with the pedestrian lights on the street. Lighting pole should be metal material and in black color.

5.3 Pedestrian Lighting

5.3.1 Location

- Pedestrian lights should be installed in the center of landscaped furniture zones.
- Pedestrian lights need to be equally spaced between street trees and be installed 45 to 50 feet on-center.

5.3.2 Breakaway bases for lighting poles are required.

5.3.3 Rather than installing individual pedestrian lights, applicant should enter into the Johnson Ferry Road Pedestrian Street Light District. Cobb County DOT and Georgia Power will install and maintain pedestrian lights in accordance with the county's pedestrian street light district ordinance. Once approvals of ¼ mile of property owners are obtained, the pedestrian street light district will be in effect.

5.3.4 Decorative lights fixture should be same as the one required in District One (See page 15-16).

6. Landscaping

All landscaping within right of way will require approval of Cobb DOT.

6.1 Center medians between opposite traffic lanes and in the center of the road

6.1.1 Convert five (5) foot center concrete medians into landscaped median covered with grass and/or shrubs

6.1.2 The center median will become a safety island when intersecting with the crosswalk (See Safety Island image on page 12).

6.2 Landscape Strip

6.2.1 Six (6) foot planting strips are located between curb or edge of the traffic lane and the sidewalk or the multiuser trail.

6.2.2 Planting strips shall be landscaped with grass or low flowering perennial shrubs.

6.3 Landscaped Furniture Zone

6.3.1 The five (5) foot furniture zone is located behind the street right-of-way and in the front of the private properties.

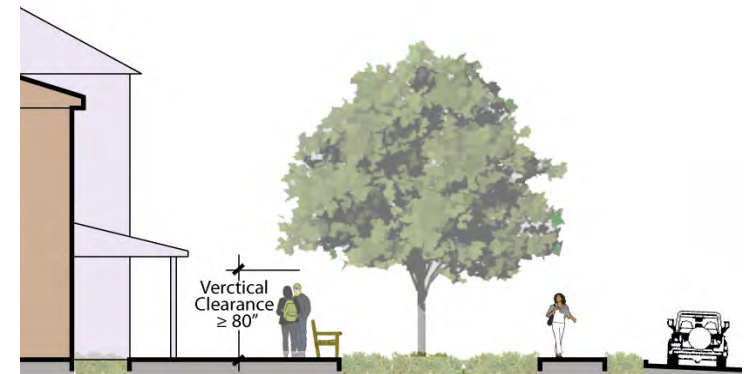
6.3.2 Grasses and street trees should be planted in this zone. Bushes and flowers are also encouraged in the area (See section 6.6 for planting species details on next page).

6.4 Street Tree

6.4.1 Street trees shall be planted in the center of the green furniture zones.

6.4.2 Street trees shall be planted with 45 to 50 feet equal separation on both sides of the street. They should be equally spaced between street lights (See Street Section Template on page 30).

6.4.3 The minimum vertical clearance of tree branches is 80 inches.



Street Tree Vertical Clearance



Streetscape Design Guidelines

6.5 Parking Lot Landscaping

Same as District One (See page 18).

6.6 Tree species and planting techniques should be selected to create a unified image of the street. Recommended plant species are:

6.6.1 Large Street Trees:

Same as District One (See page 19).

6.6.2 Landscape Buffer (in the furniture zone between building front parking lots and the street, in the landscape strip behind the street):

Same as District One (See page 20).

6.6.3 Landscape Buffer (between the street and residential properties):

Same as District One (See page 21).

6.6.4 Small Trees (in the strip between parking lot and roadway or building and the strip in parking lot):

Same as District One (See page 22).

6.6.5 Flowering Perennial Shrubs for aesthetic improvements:

Same as District One (See page 23).

7. Parking

7.1 Primary parking space shall be located on the back or the side of buildings.

7.2 Store Front Parking

7.2.1 There shall be one row of the building front parking allowed. The parking space can be at 90 degree or parallel parking depending on the availability of the building setback space. These parking should be used for temporary or short time loading/unloading parking only.

7.2.2 Parklets are encouraged in the buffer area to provide a public gathering place, especially in front of a restaurant, café or coffee house.

Parklets can be temporary and just during the short term or seasonal (See pictures in next page as the reference).

7.3 Parking Structure

Same as District One (See page 24).



Parklets



Parklets

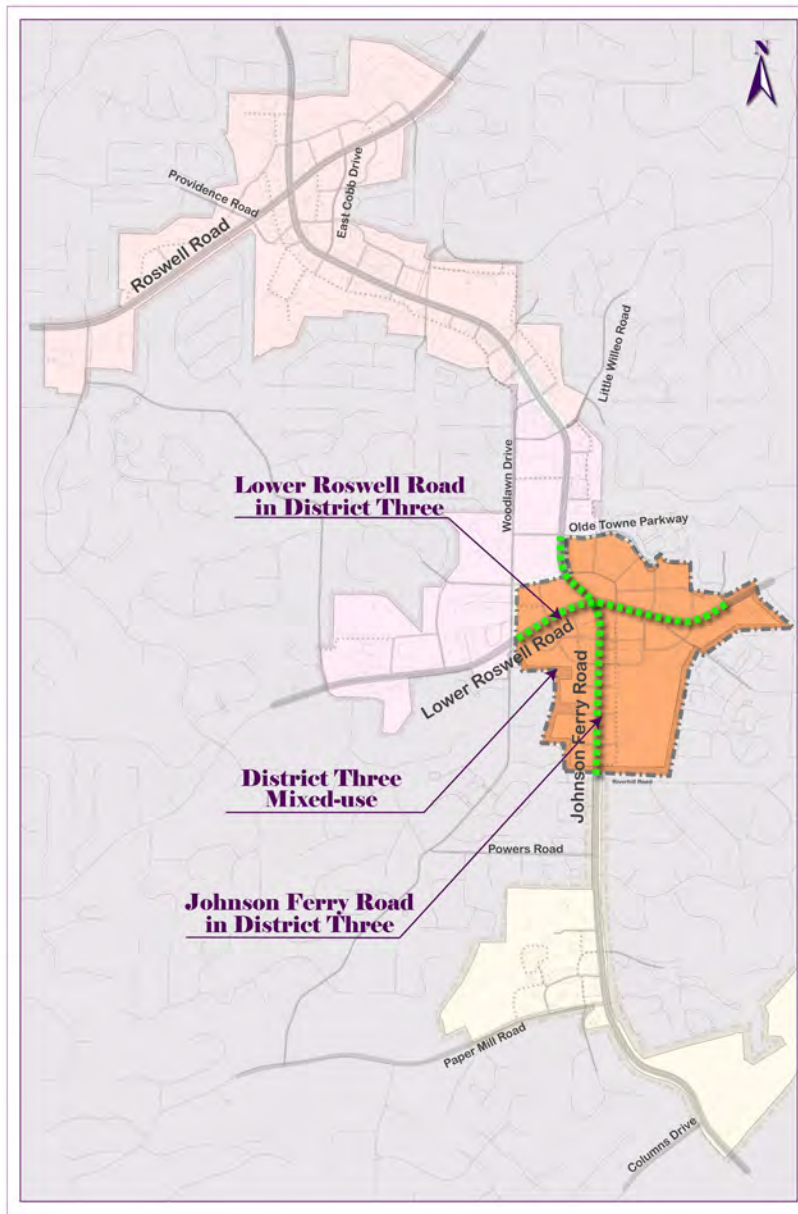
8. Utility Location

- 8.1 Utility lines should be located only on one side of the street.
- 8.2 In the long term, utility poles is recommended to be located inside of the furniture zone or sidewalk area.
- 8.3 Matching the material of utility poles to that of street lighting and pedestrian lighting is strongly recommended.
- 8.4 Utility lines crossing Johnson Ferry Road and connecting to each property should be buried underground.
- 8.5 Main utility lines are encouraged to be buried underground.

9. Building Requirement

- 9.1 Building placement and the space between the furniture zone and the building
 - 9.1.1 The 20 to 30 foot building setback could incorporate one row of parking space behind the furniture zone where it does not conflict with zoning requirements.
 - 9.1.2 The pattern and color of the pavement in front of the building is encouraged to be in conjunction with that of sidewalk or multiuser trail.
 - 9.1.3 Use of canopies and awnings on storefront and office windows is permitted and strongly encouraged. Canopies and awnings should be at least 90 inches in height as the minimum vertical clearance when they are above the walkway.
- 9.2 Architecture Standard – apply on new commercial buildings or major renovations
Same as District One (See page 25).
- 9.3 Car related Business – gas station, car wash and car repair
Same as District One (See page 26-27).





D3

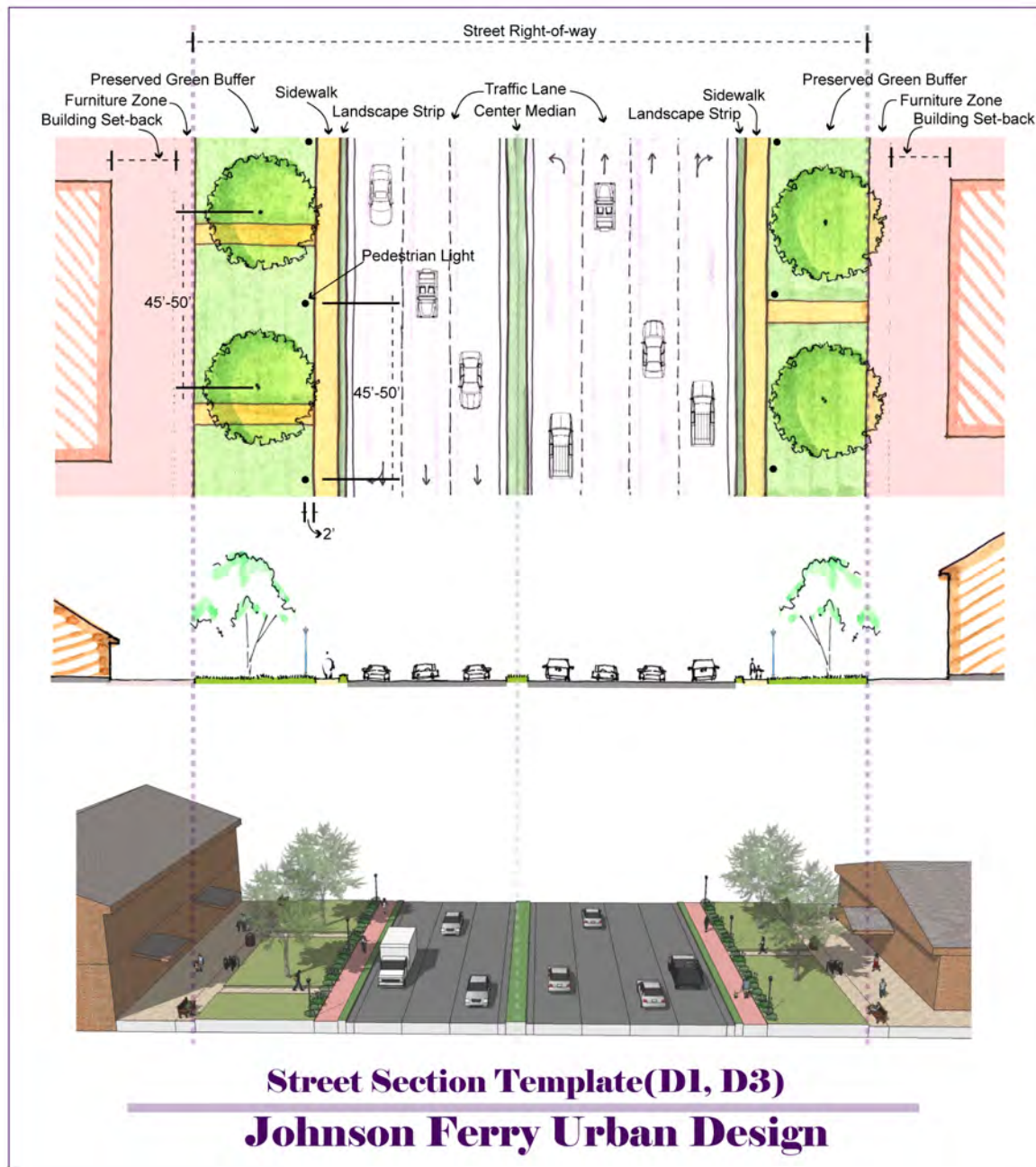
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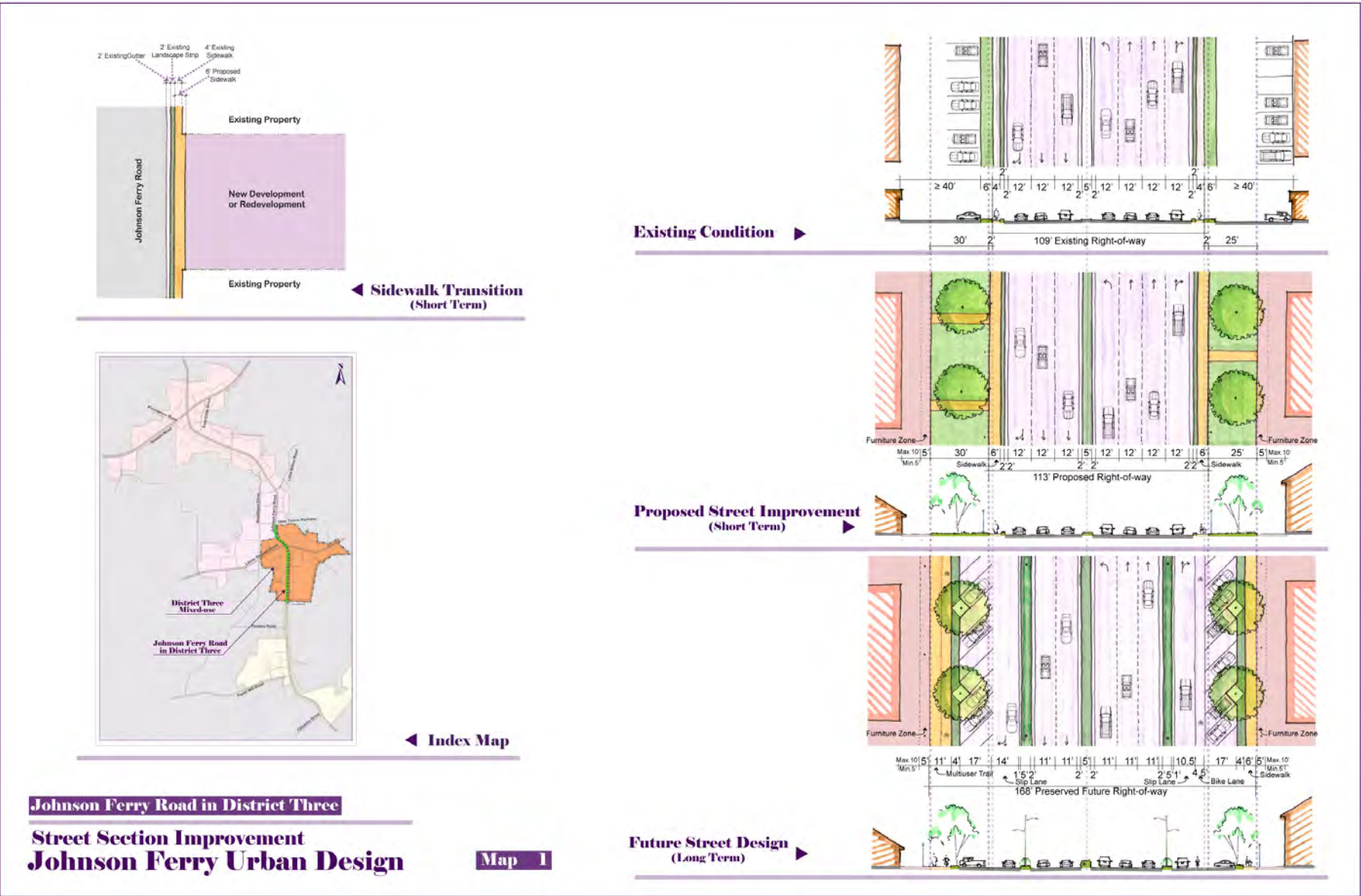


Streetscape Design Guidelines

1. Street Section Template



1. Street Section Design Graphics



Johnson Ferry Road in District Three





Johnson Ferry Road in District Three

Street Section Improvement Johnson Ferry Urban Design

Existing Condition ▶



**Proposed
Street Improvement
(Short Term)** ▶



**Future Street Design
(Long Term)** ▶



Map 2

Johnson Ferry Road in District Three

Note :

- * **Short term:** Based on Cobb DOT's Plan, 4-foot bike lanes will be added on both side of Lower Roswell Road from Johnson Ferry Road to the east. The design guidelines incooperates this plan.
- * **Long term:** Lower Roswell Road along Baseball Field side(north west of Johnson Ferry Road and Lower Roswell Road Intersection) will have no on-street parking due to the space restriction.



Index Map

Lower Roswell Road in District Three

**Street Section Improvement
Johnson Ferry Urban Design**

Map 1

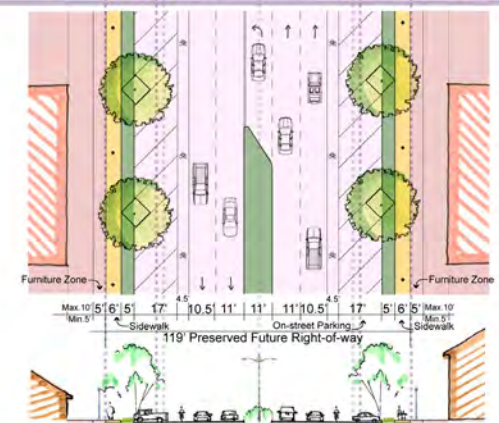
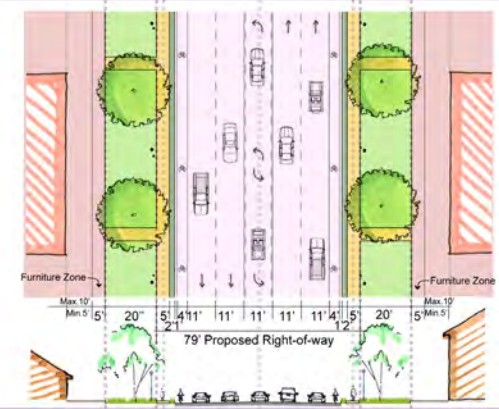


Existing Condition



**Future Street Design (Short Term)
West of Johnson Ferry Road**

**Future Street Design (Short Term)
East of Johnson Ferry Road**



**Proposed Street Improvement
(Long Term)**

Lower Rosewell Road in District Three



Streetscape Design Guidelines

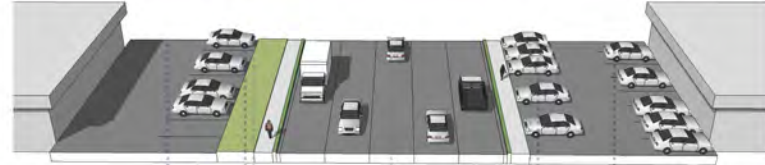
Note :

- * **Short term:** Based on Cobb DOT's Plan, 4-foot bike lanes will be added on both side of Lower Roswell Road from Johnson Ferry Road to the east. The design guidelines incorporates this plan.
- * **Long term:** Lower Roswell Road along Baseball Field side (north west of Johnson Ferry Road and Lower Roswell Road Intersection) will have no on-street parking due to the space restriction.



Lower Roswell Road in District Three

Street Section Improvement Johnson Ferry Urban Design



Existing Condition ▶



West of Johnson Ferry Road



East of Johnson Ferry Road

Proposed Street Improvement
(Short Term) ▶



Future Street Design
(Long Term) ▶

Map 2

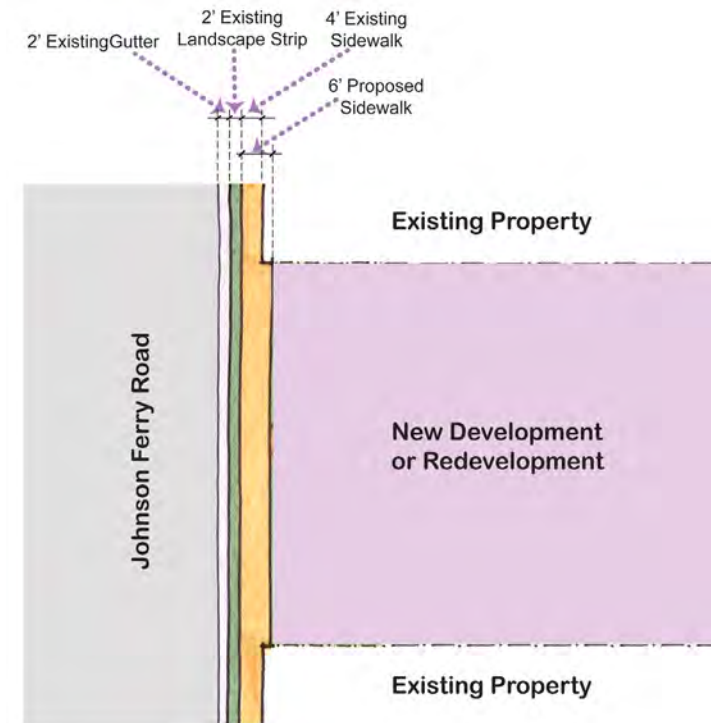
Lower Roswell Road in District Three



3. Pedestrian Facility

3.1 Sidewalk

- 3.1.1 Sidewalk shall be six (6) feet in width along the road frontages on Johnson Ferry Road and be kept as five (5) feet on Lower Roswell Road.
- 3.1.2 The location of the sidewalk is shown in the Street Section Template on page 39.
- 3.1.3 Concrete surface with DOT approved stamped concrete border to decorate and define the edges of the sidewalk is the suggested pavement style shown in the pictures below. Color shall be approved by the Community Development Agency and DOT for consistency.
- 3.1.4 On Johnson Ferry Road, if the sidewalk is in a good condition, stamped concrete should be added to extend the sidewalk two (2) feet behind the existing sidewalk. If the sidewalk is worn out or removed, a six (6) foot sidewalk should be installed.
- 3.1.5 Applying for ADA standard to meet the need of disabled individuals is required; such as, handicapped access curb cuts, blind path, etc. See images below as examples.
- 3.1.6 Sidewalk transition: the new sidewalk should have a smooth and continued transition or connection to the adjacent existing sidewalk. The recommended transitions are shown in the map beside.



Sidewalk Transition On Johnson Ferry Road



Handicapped Access Curb Cut



Blind Path



Sidewalk Pavement



Streetscape Design Guidelines

3.2 Crosswalk

The requirements are same as that in District One (See page 12).

3.3 Pedestrian Pathway

The requirements are same as that in District One (See page 13).

3.3 Bike lane (on Lower Roswell Road east of Johnson Ferry Road)

The four (4) foot bike lane shall be located in the road right of way and beside traffic lanes (See Street Section Design Graphic for the location on page 42-43). It shall be paved or stamped in the same color as sidewalk borders to clearly define the biking space and per the MUTCD.

4. Furniture Zone

The requirements are same as that in District One (See page 13-14).

5. Lighting

5.1 Street Lighting

The improvement of the Street lighting will be facilitated during the long term development period.

5.2 Parking Lot Lighting

Parking lot lighting will aid both pedestrians and motorists by creating a safe, well lit environment and add to the theme of the corridor. The lighting style should be selected in conjunction with the pedestrian lights on the street. Lighting pole should be metal material and in black color.

5.3 Pedestrian Lighting

5.3.1 Location

- Pedestrian lights should be installed two (2) feet away from the sidewalks and in preserved green buffers.
- Pedestrian lights need to be equally spaced between street trees and be installed 45 to 50 feet on-center.

5.3.2 Breakaway bases for lighting poles are required.

5.3.3 Rather than installing individual pedestrian lights, applicant should enter into the Johnson Ferry Road Pedestrian Street Light District. Cobb County DOT and Georgia Power will install and maintain pedestrian lights in accordance with the county's pedestrian street light district ordinance. Once approvals of ¼ mile of property owners are obtained, the pedestrian street light district will be in effect.

5.3.4 Decorative lights fixture should be same as the one required in District One (See page 15-16).

6. Landscaping

All landscaping within right of way will require approval of Cobb DOT.

6.1 Center medians between opposite traffic lanes and in the center of the road on Johnson Ferry Road

6.1.1 Convert five(5) foot center concrete medians into landscaped median covered with grass and/or shrubs

6.1.2 The center median will become a safety island when intersecting with the crosswalk (See Safety Island image on page 12).

6.2 Landscape Strip

6.2.1 Two (2) foot planting strips are located between curb or edge of the traffic lane and the sidewalk on Johnson Ferry Road.

6.2.2 One (1) foot planting strips to be kept on Lower Roswell Road west of Johnson Ferry Road.

6.2.3 the landscape strip on Lower Roswell Road east of Johnson Ferry Road should be widened to two (2) feet, which is consistent with DOT's bike lane plan along Lower Roswell Road.

6.2.4 Planting strips shall be landscaped with grass or low flowering perennial shrubs if applicable.

6.3 Preserved Green Buffers Behind Sidewalks

6.3.1 On Johnson Ferry Road, the 30 foot preserved green buffer is behind the sidewalks on the west side, and the 25 foot buffer is on the east side of the road for the purpose of not only creating more green space along streets in the short term but also preserving the space for future street right-of-way.



Parklets



Parklets



Streetscape Design Guidelines

- 6.3.2 On Lower Roswell Road west of Johnson Ferry Road, there are 22.5 foot preserved green buffers behind sidewalks on both sides of the street. On Lower Roswell Road east of Johnson Ferry Road, the green buffer shall be 20 feet in width.
- 6.3.3 Grasses and street trees should be planted in the buffer. Bushes and flowers are also encouraged in the area (See section 6.6 for street planting species details).
- 6.3.4 In the short term, temporary parking will be allowed as the method of compensating loss of existing parking spaces in the building front.
- 6.3.5 Parklets are encouraged in the buffer area to provide a public gathering place, especially in front of a restaurant, café or coffee house. Parklets can be temporary and just during the short term or seasonal.

6.4 Street Tree

- 6.4.1 Street trees shall be located within the preserved green buffers and 15 feet behind sidewalks on Johnson Ferry Road.
- 6.4.2 Street trees shall be located within the preserved green buffers and nine (9) feet behind sidewalks on Lower Roswell Road.
- 6.4.3 Street trees shall be planted with 45 to 50 feet equal separation on both sides of the street. They should be equally spaced between street lights (See Street Section Template in page 39).
- 6.4.4 The minimum vertical clearance of tree branches is 80 inches.

6.5 Parking Lot Landscaping

Same as District One (See page 18).

6.6 Tree species and planting techniques should be selected to create a unified image of the street. Recommended plant species are:

Same as District One (See page 19-23).

7. Parking

7.1 Primary parking space shall be located on the back or the side of buildings.

7.2 Store Front Parking

Same as District One (See page 24).

7.3 Parking Structure

Same as District One (See page 24).



8. Utility Location

- 8.1 Utility lines should be located only on one side of the street.
- 8.2 In the long term, utility poles is recommended to be located in the landscape strip between the sidewalk and the on-street parking.
- 8.3 Matching the material of utility poles to that of street lighting and pedestrian lighting is strongly recommended.
- 8.4 Utility lines crossing Johnson Ferry Road and Lower Roswell Road as well as connecting to each property should be buried underground.
- 8.5 Main utility lines are encouraged to be buried underground.

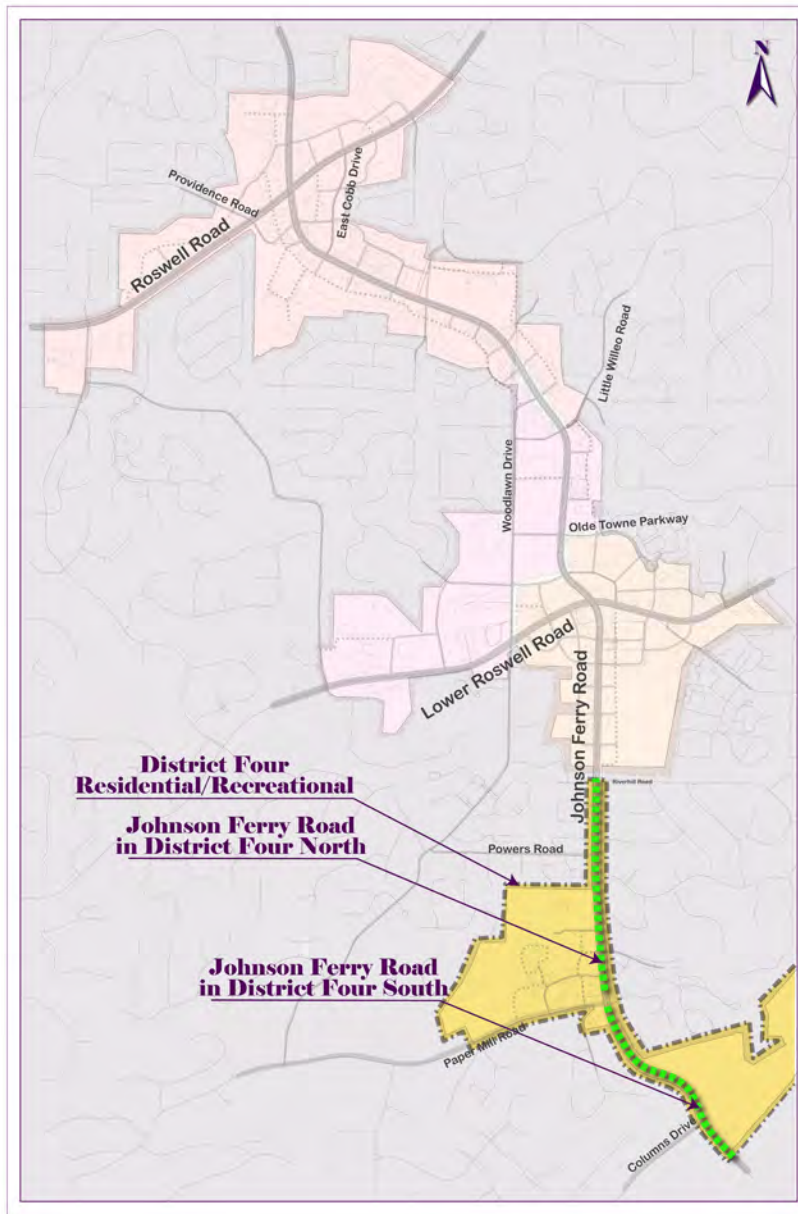
9. Commercial Building Requirement

- 9.1 Building placement and the space between the furniture zone and the building
 - 9.1.1 The five (5) to ten (10) foot building setback should be designed as the pedestrian walkway in front of buildings.
 - 9.1.2 The pattern and color of the paved pedestrian walkway in conjunction with that of sidewalk or multiuser trail is encouraged.
 - 9.1.3 It is recommended to plant small tree, bushes, grasses, or potted vegetation in front of the building to incorporate streetscape changes and provide more green space and pervious surface along the long continued walkway.
 - 9.1.4 Use of canopies and awnings on storefront and office windows is permitted and strongly encouraged. Canopies and awnings should be at least 90 inches in height as the minimum vertical clearance when they are above the walkway.
- 9.2 Architecture Standard – apply on new buildings or major renovations
 - 9.2.1 It is required that the front of all buildings along Johnson Ferry Road or Lower Roswell Road be oriented toward the street. Building placement should consider site circulation and blend with the commercial characteristics of this district.
 - 9.2.2 It is preferred that new commercial developments be kept to a neighborhood scale. The building façade color and material should be consistent with the surrounding building environment. To prevent large vertical or horizontal blank expanses on façades, architectural detailing is encouraged. Expanses can be broken up with windows, bays, materials, extending or stepping back façade, front porches or patios, balconies, alternating rooflines, awnings, landscaping or other similar features.
 - 9.2.3 Building entrance should face to and be visible from the street and directly accessible from the pedestrian sidewalk area.
- 9.3 Car related Business – gas station, car wash and car repair
Same as District One (See page 26-27).

10. Residential Building Frontage

- 10.1 In front of residential buildings a minimum 25 foot building set back is required. A fence made of brick or stone on the residential property is highly recommended.
- 10.2 The front yard of the residential property should be well landscaped.
- 10.3 In spite of on-street parking which will not be required in front of residential properties, the street right of way design in front of residential properties should remain the same as that in front of commercial properties.





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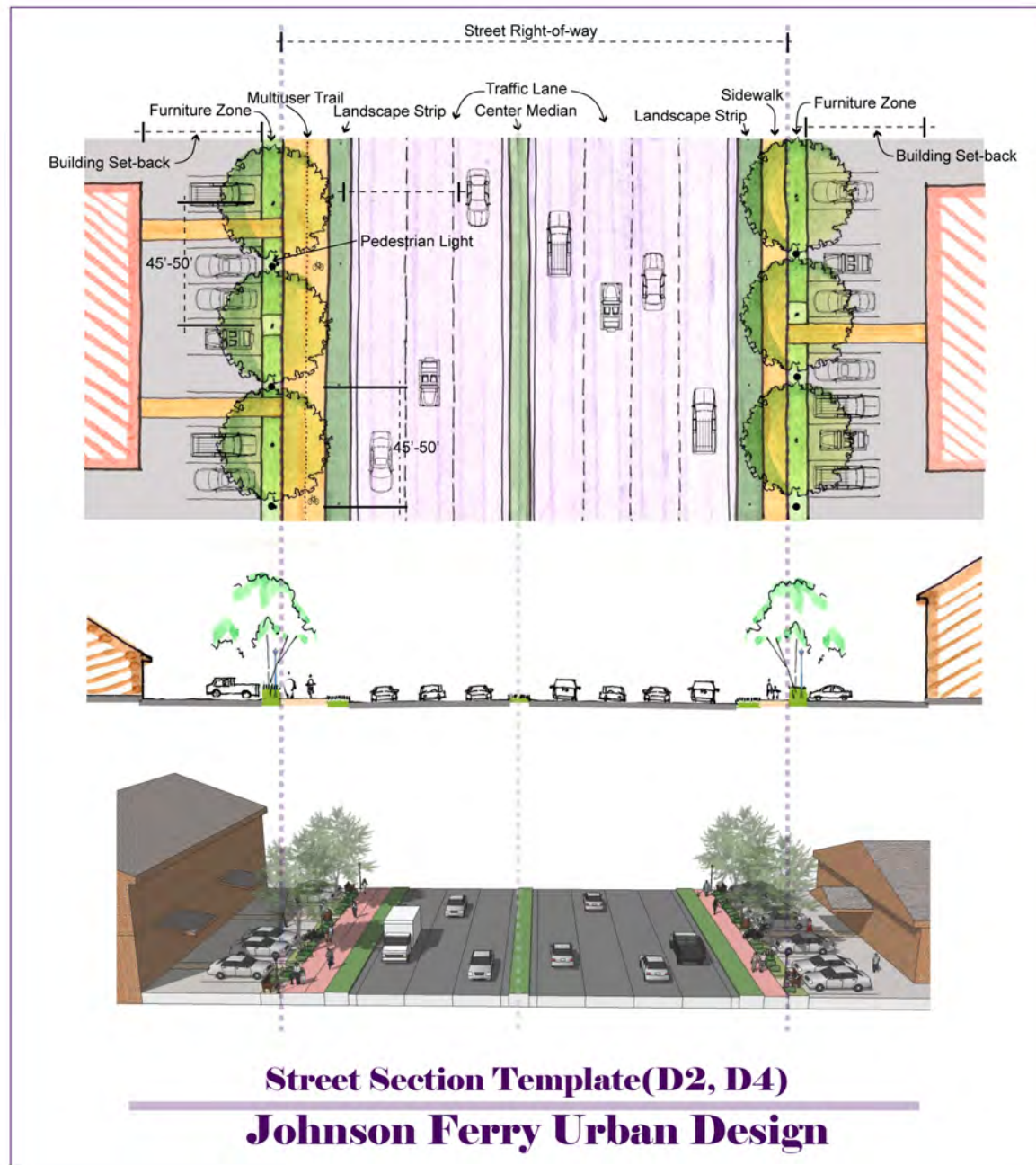
D4

District Two: Residential/Recreational



Streetscape Design Guidelines

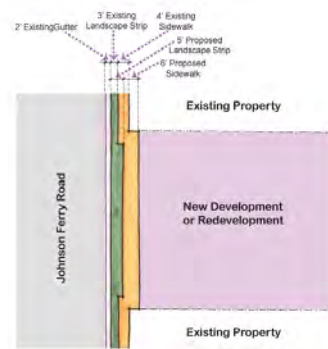
1. Street Section Template



2. Street Section Design Graphics



▲ Sidewalk Transition (Short Term)
(Westside of Johnson Ferry Road)



▲ Sidewalk Transition (Short Term)
(Eastside of Johnson Ferry Road)

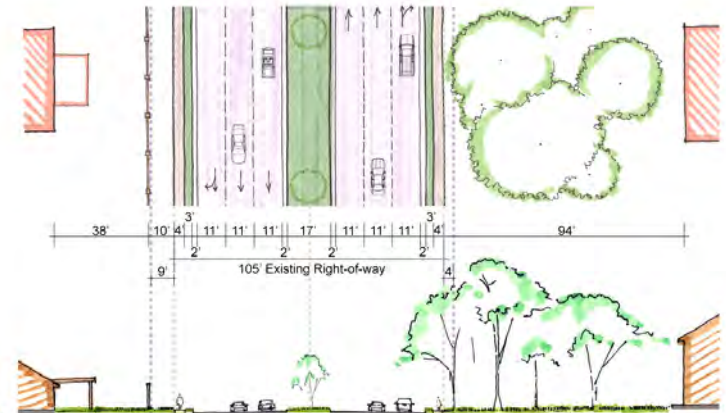


◀ Index Map

Johnson Ferry Road in District Four North (From River Hill Road to Paper Mill Road) Street Section Improvement Johnson Ferry Urban Design

Map 1

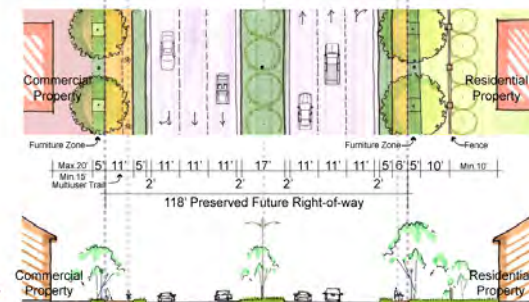
Existing Condition ▶



Proposed Street Improvement
(Short Term) ▶



Future Street Design
(Long Term) ▶





**Johnson Ferry Road in District Four North
(From River Hill Road to Paper Mill Road)**

Street Section Improvement Johnson Ferry Urban Design

Existing Condition ▶



**Proposed
Street Improvement
(Short Term)** ▶



**Future Street Design
(Long Term)** ▶



Map 2



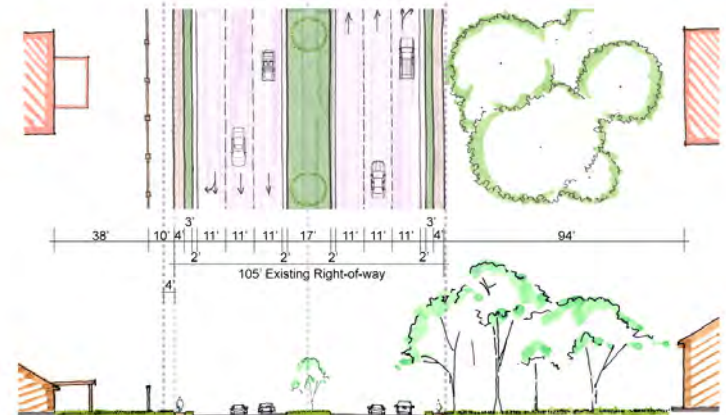


Index Map

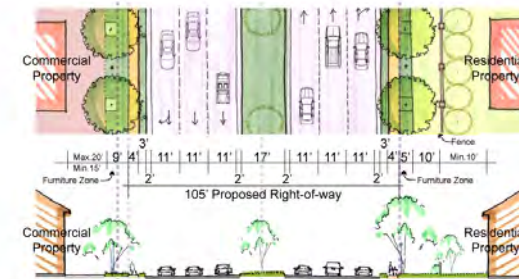
**Johnson Ferry Road in District Four South
(From Paper Mill Road to Chattahoochee River)**
**Street Section Improvement
Johnson Ferry Urban Design**

Map 1

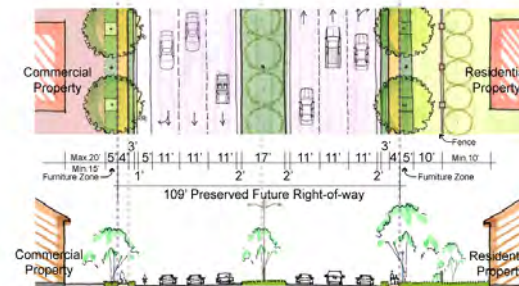
Existing Condition



Proposed
Street Improvement
(Short Term)



Future Street Design
(Long Term)





Johnson Ferry Road in District Four South
 (From Paper Mill Road to Chattahoochee River)
Street Section Improvement
Johnson Ferry Urban Design

Existing Condition ▶



Proposed Street Improvement (Short Term) ▶



Future Street Design (Long Term) ▶



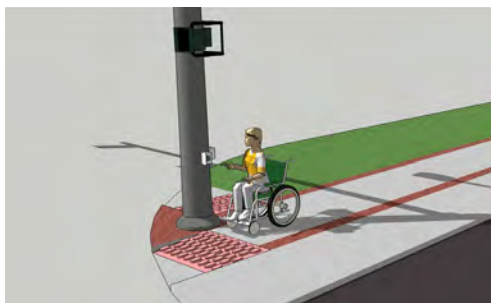
3. Pedestrian Facility

3.1 Sidewalk

- 3.1.1 Sidewalk shall be six (6) feet in width along east side of Johnson Ferry Road north of Paper Mill Road and Four (4) feet south of Paper Mill Road.
- 3.1.2 The location of the sidewalk is showed in the Street Section Template on page 50.
- 3.1.3 Concrete surface with DOT approved stamped concrete border to decorate and define the edges of the sidewalk is the suggested pavement style shown in the pictures below. Color shall be approved by the Community Development Agency and DOT for consistency.
- 3.1.4 Applying for ADA standard to meet the need of disabled individuals is required; such as, handicapped access curb cuts, blind path, etc. See images below as examples.
- 3.1.5 Sidewalk/Multiuser trail transition: the new sidewalk/multiuser trail should have a smooth and continued transition or connection to the adjacent existing sidewalk. The recommended transitions are shown in the map beside.



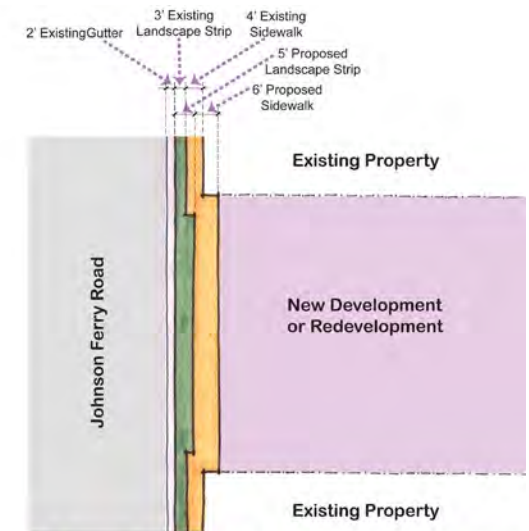
Blind Path



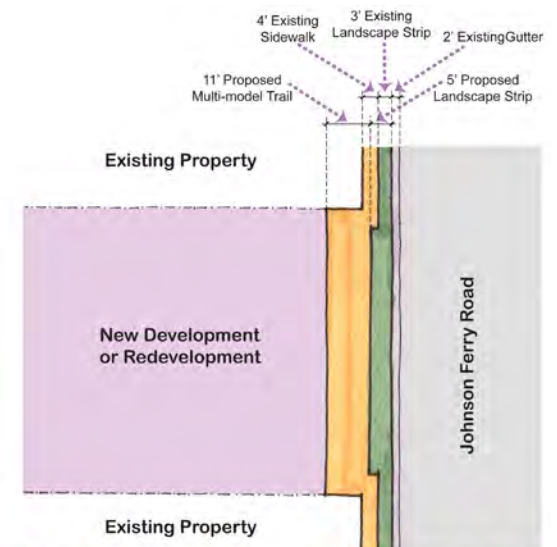
Handicapped Access Curb Cut



Sidewalk Pavement



D4 North (Eastside of Johnson Ferry Road)



D4 North (Westside of Johnson Ferry Road)

Sidewalk Transition



Streetscape Design Guidelines

3.2 Multiuser Trail

3.2.1 A eleven (11) foot multiuser trail for bicyclists and pedestrian shall be located on the west side of Johnson Ferry Road north of Paper Mill Road (See the Street Section Template on page 49 for the location).

3.2.2 The pavement style of the pedestrian walkway in the multiuser trial shall be same as the sidewalk on page 55 section 3.1.

3.2.3 The bike lane in the multiuser trail shall be paved or stamped in the same color as sidewalk borders to clearly define the biking space and per the MUTCD.

3.3 Crosswalk

The requirements are same as that in District One (See page 12).

3.4 Pedestrian Pathway

The requirements are same as that in District One (See page 13).

4. Furniture Zone

The five (5) foot furniture zone is located behind the street right-of-way and in the front of the private properties (See Street Section Template on page 50).

The furniture zone should be landscaped with grass. Street trees should be planted in this zone.

Benches, trash receptacles, and bike racks shall be placed in the furniture zone and close to the building entrance area. The Bench and the trash receptacle need to be put together as a group with no more than 100 feet spacing between each group. If the property contains less than 100 feet of frontage and more than 50 feet, a minimum of one bench and one trash receptacle is required.

The recommended furniture styles are same as those described in District One (See page 13-14).

The installation and maintenance of the furniture zone is the responsibility of private property owners or tenants of the commercial space.

5. Lighting

5.1 Street Lighting

The improvement of the Street lighting will be facilitated during the long term development period.

5.2 Parking Lot Lighting

Parking lot lighting will aid both pedestrians and motorists by creating a safe, well lit environment and add to the theme of the corridor. The lighting style should be selected in conjunction with the pedestrian lights on the street. Lighting pole should be metal material and

in black color.

5.3 Pedestrian Lighting is not required in this district.

6. Landscaping

All landscaping within right of way will require approval of Cobb DOT.

6.1 Center medians between opposite traffic lanes and in the center of the road

6.1.1 keep existing landscaping and trees in 17 foot center medians

6.1.2 The center median will become a safety island when intersecting with the crosswalk (See Safety Island image on page 12).

6.2 Landscape Strip

6.2.1 Five (5) foot planting strips on Johnson Ferry Road north of Paper Mill Road and three (3) foot planting strip on Johnson Ferry Road south of Paper Mill Road are located between curb or edge of the traffic lane and the sidewalk or the multiuser trail.

6.2.2 Planting strips shall be landscaped with grass or low flowering perennial shrubs.

6.3 Landscaped Furniture Zone

6.3.1 The five (5) foot furniture zone is located behind the street right-of-way and in the front of the private properties.

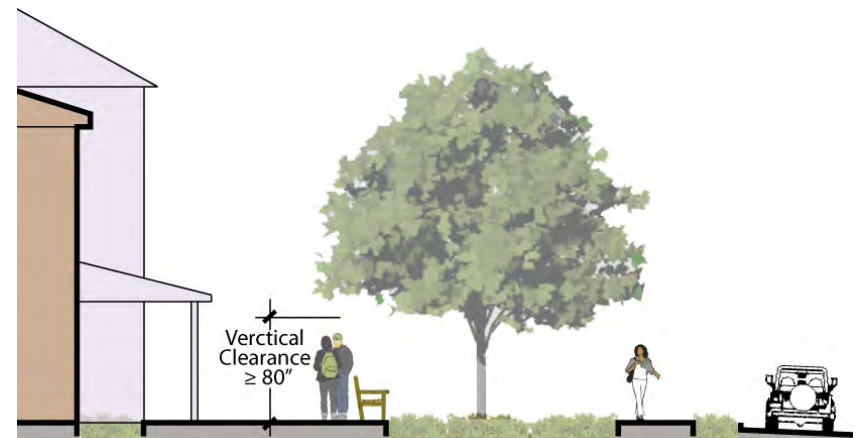
6.3.2 Grasses and street trees should be planted in this zone. Bushes and flowers are also encouraged in the area (See section 6.6 for planting species details).

6.4 Street Tree

6.4.1 Street trees shall be located in the center of the green furniture zones.

6.4.2 Street trees shall be planted with 45 to 50 feet equal separation on both sides of the street. They should be equally spaced between street lights (See Street Section Template on page 50).

6.4.3 The minimum vertical clearance of tree branches is 80 inches.



Street Tree Vertical Clearance



Streetscape Design Guidelines

6.5 Parking Lot Landscaping

Same as District One (See page 18).

6.6 Tree species and planting techniques should be selected to create a unified image of the street. Recommended plant species are:

6.6.1 Large Street Trees:

Same as District One (See page 19).

6.6.2 Landscape Buffer (in the furniture zone between building front parking lots and the street, in the landscape strip behind the street):

Same as District One (See page 20).

6.6.3 Landscape Buffer (between the street and residential properties):

Same as District One (See page 21).

6.6.4 Small Trees (in the center median, in the strip between parking lot and roadway or building and the strip in parking lot):

Same as District One (See page 22).

6.6.5 Flowering Perennial Shrubs for aesthetic improvements:

Same as District One (See page 23).

7. Parking

7.1 Primary parking space of commercial buildings shall be located on the back or the side of buildings.

7.2 Store Front Parking

7.2.1 There shall be one row of the building front parking in front of commercial allowed. The parking space can be at 90 degree or parallel parking depending on the availability of building setback space. These parking should be used for temporary or short time loading/unloading parking only.

7.2.2 Parklets are encouraged in this area to provide public gathering place especially in front of the restaurant, café or coffee house. Parklets can be temporary and just during the short term or seasonal (See pictures in next page as the reference).

7.3 Parking Structure

Same as District One (See page 24).



8. Utility Location

Parklets

- 8.1 Utility line should be placed only on one side of the street.
- 8.2 In the long term, utility poles is recommended to be located in the 4 foot landscape strip between the sidewalk and the on-street parking.
- 8.3 Matching the material of utility poles to that of street lighting and pedestrian lighting is strongly recommended.
- 8.4 Utility lines crossing Johnson Ferry Road and connecting to each property should be buried underground.
- 8.5 Main utility lines are encouraged to be buried underground.

Parklets

9. Building Requirement

- 9.1 Building placement and the space between the furniture zone and the building
 - 9.1.1 In front of the commercial building, The 20 to 25 foot building setback could incorporate one row of parking space behind the furniture zone where it does not conflict with zoning requirements..
 - 9.1.2 In front of the Residential Building The minimum of 25 foot building setback is required. The fence made by brick or stone on the residential property is highly recommended.
 - 9.1.3 The pattern and color of the pavement in front of the commercial building is encouraged to be in conjunction with that of sidewalk or multiuser trail.
 - 9.1.4 The frontyard of the residential property should be well landscaped.
 - 9.1.5 Use of canopies and awnings on storefront and office windows is permitted and strongly encouraged. Canopies and awnings should be at least 90 inches in height as the minimum vertical clearance when they are above the walkway.

- 9.2 Architecture Standard – apply on new commercial buildings or major renovations

Same as District One (See page 25).



9.3 Car Related Business – gas station, car wash and car repair

Car related business should be prohibited or limited in this district. Current car business should follow the same as District One (See page 26-27).

D. Streetscape Design Standards for Secondary Streets (including East Cobb Drive, Woodlawn Drive, Little Willeo Road, Powers Road, Paper Mill Road, and Columns Drive):

1. A five (5) foot Sidewalk is required along these streets.
2. A three (3) foot green strip between the sidewalk and the curb shall be landscaped.
3. Street trees behind the sidewalk with the equal separation at 50 feet in between are strongly encouraged. The size of trees and the tree distance from the street should meet the road clearance standard of Cobb DOT.
4. Pedestrian lights are encouraged behind the sidewalk and to be equally spaced between street trees. Breakaway bases are required for the lighting poles.
5. Parking lots adjacent to residential properties or visible from public right-of-ways is encouraged to be landscaped and screened from view using shrubs and smaller trees.
6. Complying with the ADA requirements is required.

E. Implementation

1. Public sector
 - Cobb County shall be responsible for the long term retrofit of Johnson Ferry Road and Lower Roswell Road.
 - Cobb County DOT and Georgia Power will install and maintain pedestrian lights that are installed in accordance with the county's pedestrian street light district ordinance. Once approvals of ¼ mile of property owners are obtained, the pedestrian street light district will be in effect for that segment.
 - If a county project removes a segment of the sidewalk, the sidewalk should be installed by the county in compliance with the design guidelines if budget and right of way allows.
2. Private sector
 - Property owners are responsible for the improvement of the area outside of the street right-of-way.
 - Landscape buffer, sidewalk, street trees, and furniture zone will be installed and maintained by property owners when new development or redevelopment occurs.
 - If property owner removes a segment of the sidewalk, the sidewalk should be installed by the owner in compliance with the design guidelines.



F. Street Section Design Parameter Table

Street Section	Phasing	3D View	Traffic Lane		Median	On-street Parking	Landscape Strip (Between lanes and sidewalk)	Bike Lane	Sidewalk	Preserved Green Buffer	Furniture Zone	Building Set-back	Street Right-of-Way
			Pass-through Traffic Lane	Slip Lane For Local Traffic									
D1	Existing		Lane width: 12 feet, 3 lanes on each direction, 1-2 left-turn lanes and 0-1 right-turn lane at major intersections	No	5 feet concrete	No	2 feet, covered with grass	No	4 feet concrete	No	No	not less than 40 feet, more than 100 feet in major shopping center	109 feet
	Short Term		Lane width: 12 feet, 3 lanes on each direction, 1-2 left-turn lanes and 0-1 right-turn lane at major intersections	No	5 feet green median covered with grass and/or shrubs, flowers	No	2 feet, covered with grass and/or shrubs, flowers	No	6 feet stamped concrete border color	30' on west side, 25' on east side	5 feet pavements on both side, behind Preserved green buffers, and in private zone properties	10-15 feet behind the furniture zone	113 feet
	Long Term		Lane width: 11 feet, 2 lanes on each direction, 1-2 left-turn lanes and 0-1 right-turn lane at major intersections	Lane width: 14 feet, 1 lane on each direction, 1-2 left-turn lanes and 0-1 right-turn lane at major intersections beside the on-street parking	9 feet green median between two-way traffic lanes, covered with grass, bushes, and flowers, 3 feet green median between pass-through traffic lanes and slip lane, covered with grass and bushes	Yes, behind the slip lane	4 feet, covered with grass, bushes, and big shade street trees	West side: 5 feet bike lane within 11 feet multiuser trail, East side: 4.5 feet bike lane between slip lane and on-tree parking	West side: sidewalk within 11 feet multiuser trail, East side: 6 feet sidewalk, concrete with color stamped concrete borders	No	5 feet pavements on both side, behind sidewalks and in private properties	10-15 feet behind the furniture zone	168 feet
D2	Existing		Lane width: 12 feet, 3 lanes on each direction, 1 left-turn lane, 0-1 right-turn lane at major intersections	No	5 feet concrete	No	2 feet, covered with grass	No	4 feet concrete	No	No	not less than 40 feet	109 feet
	Short Term		Lane width: 12 feet, 3 lanes on each direction, 1 left-turn lane, 0-1 right-turn lane at major intersections	No	5 feet green median covered with grass and/or shrubs, flowers	No	6 feet, covered with grass and/or shrubs, flowers	West side 5 feet bike lane within 11 feet multiuser trail	West side: 6 feet sidewalk within 11 feet multiuser trail, East side: 6 feet sidewalk, concrete with color stamped concrete border	No	5 feet green furniture zone on both side behind the sidewalk cover with grass, bushes, and big shade street trees	20-30 feet behind the furniture zone	126 feet
	Long Term		Lane width: 11 feet, 3 lanes on each direction, 1 left-turn lane, 0-1 right-turn lane at major intersections	No	12 feet green median covered with grass, bushes and flowers	No	6 feet, covered with grass, bushes, and flowers	West side 5 feet bike lane within 11 feet multiuser trail	West side: 6 feet sidewalk within 11 feet multiuser trail, East side: 6 feet sidewalk, concrete with color stamped concrete border	No	5 feet green furniture zone on both side behind the sidewalk cover with grass, bushes, and big shade street trees	20-30 feet behind the furniture zone	126 feet
D3	Existing		Lane width: 12 feet, 3 lanes on each direction, 1-2 left-turn lanes, 0-1 right-turn lane at major intersections	No	5 feet concrete	No	2 feet, covered with grass	No	4 feet concrete	No	No	not less than 40 feet, more than 100 feet in major shopping center	109 feet
	Short Term		Lane width: 12 feet, 3 lanes on each direction, 1-2 left-turn lanes, 0-1 right-turn lane at major intersections	No	5 feet green median covered with grass and/or shrubs, flowers	No	2 feet, covered with grass and/or shrubs, flowers	No	6 feet stamped concrete border color	30' on west side, 25' on east side	5 feet pavements on both side, behind preserved green buffers, and in private zone properties	5-10 feet behind the furniture zone	113 feet
	Long Term		Lane width: 11 feet, 3 lanes on each direction, 1-2 left-turn lanes, 0-1 right-turn lane at major intersections	Lane width: 14 feet, 1 lane on each direction, 1-2 left-turn lanes, 0-1 right-turn lane at major intersections beside the on-street parking	5 feet green median between two-way traffic lanes covered with grass and bushes, 5 feet green median between pass through traffic lanes and slip lane covered with grass, bushes and understorey trees	Yes, behind the slip lane	4 feet, covered with grass, flowers, bushes, and big shade street trees	West side: 5 feet bike lane within 11 feet multiuser trail, East side: 4.5 feet bike lane between slip lane and on-tree parking	West side: 6 feet sidewalk within 11 feet multiuser trail, East side: 6 feet sidewalk, concrete with color stamped concrete border	No	5 feet pavements on both side, behind sidewalks and in private properties	5-10 feet behind the furniture zone	168 feet
D4-North	Existing		Lane width: 11 feet, 3 lanes on each direction, 1 left-turn lane, 0-1 right-turn lane at major intersections	No	17 feet median covered with grass and few small trees	No	3 feet covered with grass	No	4 feet concrete	No	No	not less than 40 feet for housing in subdivision, more than 70 feet for each individual housing	105 feet
	Short Term		Lane width: 11 feet, 3 lanes on each direction, 1 left-turn lane, 0-1 right-turn lane at major intersections	No	17 feet median covered with grass and few small trees	No	5 feet covered with grass and/or flowers, shrubs	west side, 5 feet bike lane within 11 feet multiuser trail	West side: 6 feet sidewalk within 11 feet multiuser trail, East side: 6 feet sidewalk, concrete with color stamped concrete border	No	5 feet green furniture zone on both side behind the sidewalk cover with grass, bushes, and big shade street trees	Commercial side: 15-20 feet, Residential side: 20 feet at the minimum	118 feet
	Long Term		Lane width: 11 feet, 3 lanes on each direction, 1 left-turn lane, 0-1 right-turn lane at major intersections	No	17 feet green median covered with grass shrubs, flowers and dense understorey street trees	No	5 feet covered with grass, flowers and bushes	west side, 5 feet bike lane within 11 feet multiuser trail	West side: 6 feet sidewalk within 11 feet multiuser trail, East side: 6 feet sidewalk, concrete with color stamped concrete border	No	5 feet green furniture zone on both side behind the sidewalk cover with grass, bushes, and big shade street trees	Commercial side: 15-20 feet, Residential side: 20 feet at the minimum	118 feet
D4-South	Existing		Lane width: 11 feet, 3 lanes on each direction, 1 left-turn lane, 0-1 right-turn lane at major intersections, change to 2-way 4-lane right before Chattahoochee bridge, south of Columns Drive	No	17 feet median covered with grass and few small trees	No	3 feet covered with grass	No	4 feet concrete	No	No	not less than 40 feet for housing in subdivision, more than 70 feet for each individual housing	105 feet
	Short Term		Lane width: 11 feet, 3 lanes on each direction, 1 left-turn lane, 0-1 right-turn lane at major intersections	No	17 feet median covered with grass and few small trees	No	3 feet covered with grass and/or flowers, shrubs	No	4 feet sidewalk on both sides, concrete with color stamped concrete border	No	9 feet green furniture zone on west side and 5 feet on east side cover with grass, bushes, and big shade street trees	Commercial side: 15-20 feet, Residential side: 20 feet at the minimum	105 feet
	Long Term		Lane width: 11 feet, 3 lanes on each direction, 1 left-turn lane, 0-1 right-turn lane at major intersections	No	17 feet green median covered with grass, flowers, shrubs and dense understorey street trees	No	3 feet covered with grass, flowers, and bushes	west side, 5 feet bike lane between traffic lanes and landscape strips	4 feet sidewalk on both sides, concrete with color stamped concrete border	No	5 feet green furniture zone on both side cover with grass, bushes, and big shade street trees	Commercial side: 15-20 feet, Residential side: 20 feet at the minimum	109 feet
D3	Existing		Lane width: 12 feet, 2 lanes on each direction, 1 left-turn only lane for both direction through the street	No	No	Np	1 feet covered with grass	No	5 feet concrete	No	No	not less than 45 feet	74 feet
	Short Term		Lane width: 12 feet from Johnson Ferry to westland 11 feet from Johnson Ferry to east, 2 lanes on each direction, 1 left-turn only lane for both direction through the street	No	No	No	1 feet from Johnson Ferry to west and 2 feet from Johnson Ferry to east, covered with grass	4 feet bike lane on both sides of the street from Johnson Ferry to east	5 feet sidewalk on both sides, concrete with color stamped concrete border	22.5 feet on both sides of the street from Johnson Ferry to west and 20 feet from Johnson Ferry to east	5 feet pavements on both side, behind preserved green buffers, and in private zone properties	5-10 feet behind the furniture zone	74 feet from Johnson Ferry to west, 79 feet from Johnson Ferry to east
	Long Term		One 11 feet lane and one 10.5 feet slow traffic lane on each direction, 1 left-turn lane, 0-1 right-turn lane	No	Convert existing 12' Left-turn left to green median covered with grass, bushes and flowers. Keep the left turn lane only at the major intersections	Yes, both sides of the street	5 feet covered with grass, flowers, and shrubs	4.5 feet bike lane between on-street parking and slow traffic lane on both side of the street	6 feet behind the landscape strip, concrete with color stamped concrete border	No	5 feet pavements on both side, behind sidewalks and in private properties	5-10 feet behind the furniture zone	119 feet

G. Street Improvement - Short Term

